



City of Huntington Beach Planning and Building Department

STAFF REPORT

TO: Planning Commission
FROM: Scott Hess, AICP, Director of Planning and Building
BY: Tess Nguyen, Associate Planner *TN*
DATE: November 27, 2012

SUBJECT: SITE PLAN REVIEW NO. 2012-003, CONDITIONAL USE PERMIT NO. 2012-009, VARIANCE NO. 2012-004 (SKATE PARK PROJECT)

APPLICANT: VF Outdoor, Inc., 6550 Katella Avenue, Cypress, CA 90630

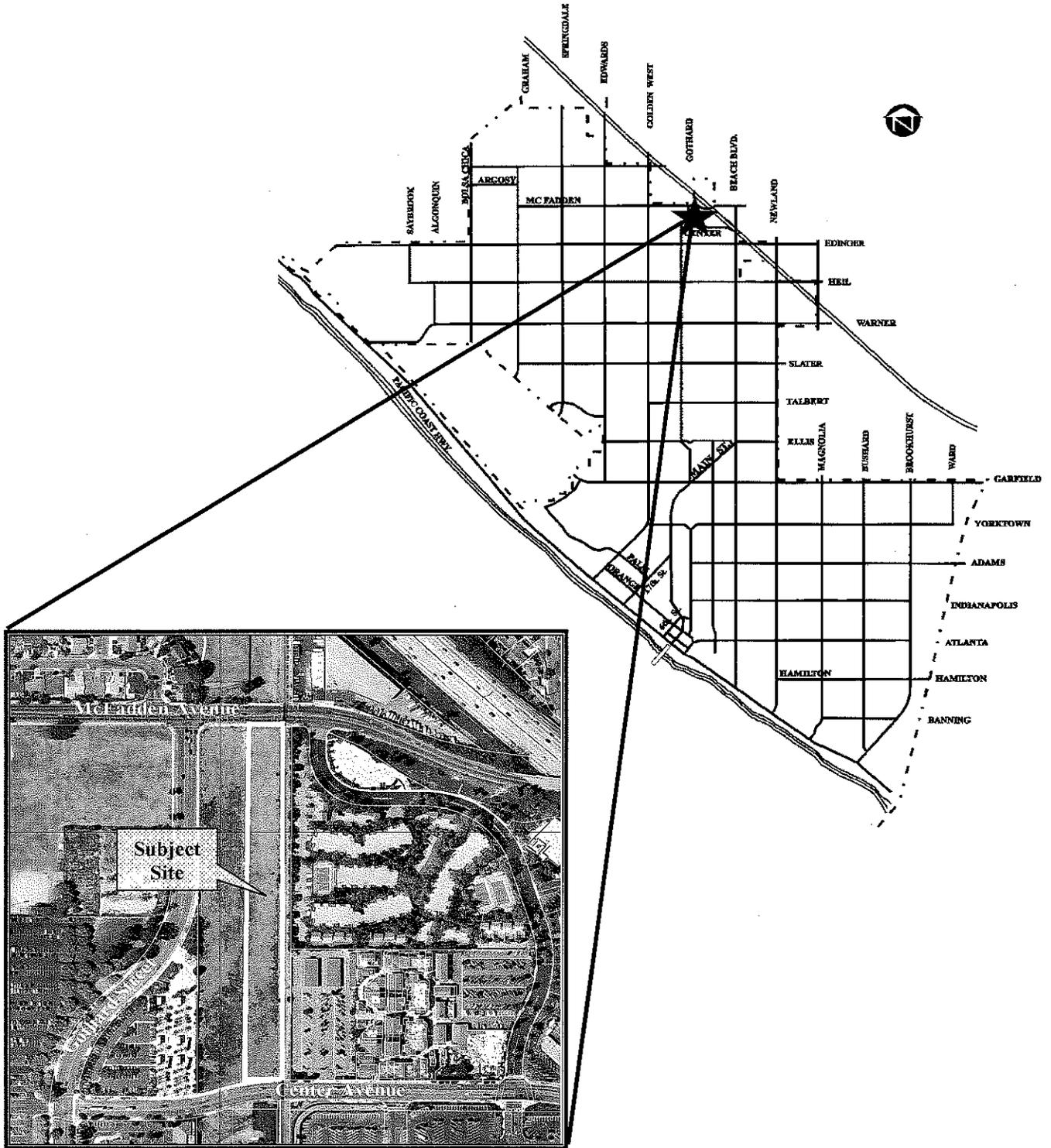
PROPERTY

OWNER: City of Huntington Beach, 2000 Main Street, Huntington Beach, CA 92648

LOCATION: 7461 Center Avenue, Huntington Beach, CA 92647 (2.7-acre vacant site on Center Avenue, approximately 500 ft. east of Gothard Street)

STATEMENT OF ISSUE:

- ◆ Site Plan Review No. 2012-004 represents a request to permit the development of a skate park consisting of a 14,500 sq. ft. skate plaza, an 11,800 sq. ft. skate bowl, a 3,500 sq. ft. skate shop, a 185 sq. ft. restroom structure, a 150 sq. ft. skate park entrance kiosk, and 26 parking spaces.
- ◆ Conditional Use Permit No. 2012-009 represents a request to permit the on-site reduction of required parking and to permit off-site parking at the Huntington Beach Sports Complex during special events.
- ◆ Variance No. 2012-004 represents a request to permit deviations in the distance of joint use parking during special events, driveway width, driveway setback, parking lot landscaping screening, frontage street lighting, and frontage coverage.
- ◆ Staff's Recommendation:
Approve Site Plan Review No. 2012-004, Conditional Use Permit No. 2012-009, and Variance No. 2012-004 with modifications based upon the following:
 - Facilitates the development of a public skate park to provide additional recreational opportunities for the community.
 - Improves the City's fiscal viability by stimulating tourism and increasing tax revenues in conjunction with the skate park special events.
 - Complements a diversity of uses within the Beach and Edinger Corridors Specific Plan (BECSP) area, particularly along the Center Avenue corridor.
 - Consistent with good zoning practice and implements the goals the BECSP while conforming to the standards and regulations set forth in the development code.
 - Compatible with existing and anticipated land uses.



VICINITY MAP
SITE PLAN REVIEW NO. 2012-003/CONDITIONAL USE PERMIT NO. 2012-009/VARIANCE NO. 2012-004
(SKATE PARK PROJECT—7461 CENTER AVENUE)

♦ Staff Recommended Modifications:

Site Plan Review No. 2012-003, Conditional Use Permit No. 2012-009, and Variance No. 2012-004

- Require the primary volume of the south facing building façade to incorporate a major offset, notch, and façade composition change.
- Require the ground level retail shopfront of the south facing building façade to contain a minimum of 70% clear-glass display windows framed within storefront pilasters and base.
- Require the metal panel base of the south facing building façade to be provided for the entire building façade facing the street.
- Require a substantial horizontal articulation of the south facing building façade to be provided at the top of the uppermost floor of the façade.
- Require the wall composition of the south facing building façade to contain a minimum of twenty (20) percent and a maximum of sixty (60) percent glazing.

RECOMMENDATION:

Motion to:

- A. “Approve CEQA Findings of Fact with a Statement of Overriding Considerations – EIR No. 10-009 (Attachment No. 7)
- B. “Approve Site Plan Review No. 2012-003, Conditional Use Permit No. 2012-009, and Variance No. 2012-004 as modified with findings and suggested conditions of approval (Attachment No. 1).”

ALTERNATIVE ACTION(S):

The Planning Commission may take alternative actions such as:

- A. “Continue Site Plan Review No. 2012-003, Conditional Use Permit No. 2012-009, and Variance No. 2012-004 and direct staff accordingly.”
- B. “Deny Site Plan Review No. 2012-003, Conditional Use Permit No. 2012-009, and Variance No. 2012-004 with findings for denial.”

PROJECT PROPOSAL:

Site Plan Review No. 2012-003 represents a request to permit the development of a skate park consisting of a 14,500 sq. ft. skate plaza, an 11,800 sq. ft. skate bowl, a 3,500 sq. ft. skate shop, a 185 sq. ft. restroom structure, a 150 sq. ft. skate park entrance kiosk, and 26 parking spaces.

Conditional Use Permit No. 2012-009 represents a request to permit the on-site reduction of required parking and to permit off-site parking at the Huntington Beach Sports Complex during special events.

Variance No. 2012-004 represents a request to permit deviations in the distance of joint use parking during special events, driveway width, parking lot setbacks, parking lot landscaping screening, frontage street lighting, and frontage coverage as follows:

- distance of joint use parking during special events: 2.8 miles in lieu of 250 ft. maximum distance between the use and the nearest point of the parking facility
- driveway width: 26 ft. wide driveway in lieu of a maximum 24 ft. wide driveway allowed
- driveway setback: two ft. setback for driveways in lieu five ft. required
- parking area landscaping screening: zero landscaping screening in lieu of three ft. required
- frontage street lighting: one light standard in lieu of two required light standards
- frontage coverage: 47% frontage coverage in lieu of minimum 90% required

The applicant (VF Outdoor, Inc.) proposes to lease the vacant property from the City to develop, maintain, and operate a public skate park, including a skate plaza area, a skate bowl area, a skate shop, a restroom structure, and an entry kiosk. The skate park proposes to be open to the public and operate seven days a week from 10:00 AM to 10:00 PM and be supervised during these hours. The retail building, which is intended to be complementary to the skate park and cater to its users, includes areas for a pre-packaged concession stand (food service), skate rental space, offices, restrooms, and storage. The retail building would operate independently of the skate park and be open only during normal business hours (10:00 AM to 8:00 PM on weekdays and 10:00 AM to 6:00 PM on weekends).

The proposed 26-space main parking lot is accessed from Center Avenue. A secondary parking area, proposed for use only during special events is accessed from McFadden Avenue. Under normal daily operations, the project site would be accessed via the primary driveway on Center Avenue for drop-off and parking for skate park users/visitors and retail patrons. During intermittent special events throughout the year, the site's secondary access driveway on McFadden Avenue would be utilized for special event employees and event vendors.

The proposed skate park would host up to 15 special event days throughout the year, expected to draw between 300 to 2,500 spectators, with overflow parking proposed to be located at the Huntington Beach Sports Complex. Twelve event days would be held on weekends and generally draw 300 to 500 spectators per day. The remaining event days would consist of one major annual three-day event expected to draw up to 2,500 spectators per day, starting on a Friday and ending on a Sunday. During these events, visitors would be directed to park their vehicles at the Huntington Beach Sports Complex and be transported to and from the skate park via shuttle buses. To ensure adequate parking for special events, the schedule of major events would only occur on days where no events are planned at the Sports Complex. The hours of operation for special events would be 10:00 AM to 10:00 PM, consistent with normal skate park operations. The project includes a public address system to be used periodically during normal daily operations. However, during special events, amplified music and announcements from the PA system would continue through the duration of the event.

Since the project site is located east of the OCTA Transportation Center and west of the Union Pacific Railroad, the site could be a potential location for a pedestrian transit stop. The City, as part of the proposed project, is recommending as a suggested condition the applicant set aside an area for the future installation of a transit platform. This area, referred to as the "Transit Reserve Area," would consist of a 20-foot wide by 300-foot long area at the southeast corner of the project site. The project applicant would not place permanent structures in the Transit Reserve Area and would remove any landscaping or pavement within the Transit Reserve Area if a transit platform were installed. The Transit Reserve Area is depicted on the site plan.

Background:

The General Plan Housing Element previously specified that the 2.7-acre site will be designated exclusively for residential uses and indicated that the City intends for the site to be developed with a minimum of 175 affordable units. In addition, the site was originally designated in the Beach and Edinger Corridors Specific Plan (Town Center – Neighborhood) as “Residential Required.” In order to permit the development of the proposed skate park, an amendment to the General Plan Housing Element and a Zoning Text Amendment to the Beach and Edinger Corridors Specific Plan to remove the housing designation on the project site was approved by the City Council on May 7, 2012.

Study Session:

The Planning Commission held a study session for the project on November 13, 2012 and discussed the following issues:

- *Distance of Special Events Parking*
Commissioner Ryan requested clarification over the proposed distance of joint use parking. According to HBZSO Section 231.06 (Joint Use Parking), the maximum distance between the use and the nearest point of the joint use parking facility is required to be 250 ft. Since the site is not able to provide onsite parking for special events, the applicant is proposing to provide parking off-site at the Huntington Beach Sports Complex, located approximately 2.8 miles from the project site. Therefore, the applicant is requesting to allow a distance of 2.8 miles in lieu of 250 ft. maximum distance between use (skate park) and the nearest point of the parking facility (Huntington Beach Sports Complex) during special events.
- *Project Plans*
Commissioner Bixby requested the project plans to be sent in an email in order review the plans in greater detail. The project plans were sent to the Planning Commission on November 14, 2012.
- *Driveway Width*
Commissioner Bixby requested clarification on the driveway width requirements. City Specification No. 401 (Minimum Standards for Fire Apparatus Access) requires that driveways shall not be less than 27 ft. wide. However, Fire Department staff has indicated that a minimum 26 ft. wide driveway would be sufficient based on the layout of the site for the proposed project. The BECSP allows a maximum width of 24 ft. for a two-lane driveway to facilitate a more vital and attractive pedestrian-oriented environment. In order to comply with City Specification No. 401, the applicant is requesting to provide a 26 ft. wide driveway in lieu of the 24 ft. wide driveway required by the BECSP.
- *Joint Use Parking Agreement*
Commissioner Bixby requested information regarding the joint use parking agreement. According to HBZSO Section 231.06 (Joint Use Parking), evidence of an agreement for joint use parking shall be provided by proper legal instrument, approved as to form by the City Attorney. The instrument shall be recorded in the Office of the County Recorder and shall be filed with the City prior to issuance of building permit and/or certificate of occupancy, whichever occurs first. The Community Services Department is drafting the Joint Use Parking Agreement for review by the City Council in early 2013.

▪ *Parking Management Plan*

Commissioner Bixby requested information regarding the Parking Management Plan for the special events activities. While adequate parking for special events is anticipated to be provided at the Huntington Beach Sports Complex, special events have the potential to create parking impacts for adjacent neighbors of the project site. Therefore, a Parking Management Plan, as described in the Analysis Section, will be required.

ISSUES:

Subject Property and Surrounding Land Use, Zoning, and General Plan Designations:

| LOCATION | GENERAL PLAN | ZONING | LAND USE |
|--|---|--|---|
| Subject Property: | M-sp-d (Mixed Use – Specific Plan Overlay – Design Overlay) | SP 14 (Beach and Edinger Corridors Specific Plan – Town Center Neighborhood) | Vacant |
| North (across McFadden Avenue in the City of Westminster) of Subject Property: | P/OS (Park/Open Space) and RL (Residential – Low) | P-F (Public Facilities) and R-1 (Single Family Residence) | Single Family Residential and College Park |
| South (across Center Avenue) of Subject Property: | M-sp-d (Mixed Use – Specific Plan Overlay – Design Overlay) | SP 14 (Beach and Edinger Corridors Specific Plan – Town Center Neighborhood) | SCE Transmission Towers, Plant Nursery Storage, and Shopping Center |
| East (across railroad tracks) of Subject Property: | M-sp (Mixed Use-Specific Plan Overlay) | SP 1 (North Huntington Center Specific Plan) | Old World Village and Multi-Family Residential |
| West of Subject Property: | M-sp-d (Mixed Use – Specific Plan Overlay – Design Overlay) | SP 14 (Beach and Edinger Corridors Specific Plan – Town Center Neighborhood) | SCE Transmission Lines and OCTA Transportation Center |

General Plan Conformance:

The project site is located within the Town Center Neighborhood segment of the Beach and Edinger Corridors Specific Plan. The General Plan Land Use Map designation of the subject property is Mixed Use-Specific Plan Overlay-Design Overlay (M-sp-d). The proposed project is consistent with the following General Plan goals, policies, and objectives:

A. *Air Quality Element*

Goal AQ 1: Improve regional air quality by a) decreasing reliance on single occupancy vehicular trips, b) increasing efficiency of transit, c) shortening vehicle trips through a more efficient job-housing balance and a more efficient land use pattern, and d) increasing energy efficiency.

Policy AQ 1.10.1: Continue to require the utilization and installation of energy conservation features in all new construction.

B. Circulation Element

Objective CE 2.3: Ensure that the location, intensity and timing of new development is consistent with the provision of adequate transportation infrastructure and standards as defined in the Land Use Element.

Goal CE 2.3.4: Require new development mitigate its impact on City streets, including but not limited to, pedestrian, bicycle, and vehicular conflicts, to maintain adequate levels of service.

Policy CE 6.1.6: Maintain existing pedestrian facilities and require new development to provide pedestrian walkways and bicycle routes between developments, schools, and public facilities.

C. Land Use Element

Goal LU 2: Ensure that development is adequately served by transportation infrastructure, utility infrastructure, and public services.

Goal LU 4: Achieve and maintain high quality architecture, landscape, and public open spaces in the City.

Goal LU 7: Achieve a diversity of land uses that sustain the City's economic viability, while maintaining the City's environmental resources and scale and character.

Objective LU 7.1: Accommodate the development of a balance of land uses that (a) provides for the housing, commercial, employment, educational, cultural, entertainment, and recreation needs of existing and future residents, (b) provides employment opportunities for residents of the City and surrounding subregion, (c) captures visitor and tourist activity, and (d) provides open space and aesthetic "relief" from urban development.

D. Recreation and Community Services Element

Goal RCS 1: Enrich the quality of life for all citizens of Huntington Beach by providing constructive and creative leisure opportunities.

Objective RCS1.1: Encourage recreational opportunities unique to Huntington Beach which will enhance visitation and economic development.

E. Urban Design Element

Goal UD 1: Enhance the visual image of the City of Huntington Beach.

Objective UD 1.3: Strengthen the visual character of the City's street hierarchy in order to clarify the City's structure and improve Citywide identity.

Policy UD 1.1.3: Require a consistent design theme and/or landscape design character along the community's corridors that reflects the unique qualities of each district. Ensure that streetscape standards for the major commercial corridors, the residential corridors, and primary and secondary image corridors provide each corridor with its own identity while promoting visual continuity throughout the City.

The project would provide a new state-of-the art skate park and associated retail use that enriches the quality of life for residents of the City, enhances economic development, allows for more efficient use of land resources, and achieves high quality public open spaces in the City. The area has a variety of complementary uses that are critical to any vibrant community such as regional commercial and entertainment uses, employment centers, a transit hub, a community college. Because of its location, the site would be appropriate in accommodating an infill development that serves as a recreational amenity to support the residential communities (i.e. Village at Bella Terra, the Boardwalk and Archstone HB Lofts mixed use projects) surrounding the project site. The project would increase the City's tax revenue through sales tax from retail sales and periodic special events. In doing so, multiple sustainable development principles are achieved, resulting in the recreational and economic well-being of the area. The project provides access to recreational opportunities for residents seeking to be within walking distance of work, entertainment, and recreation, thus reducing dependency on their automobile and having access to multiple amenities for an active and mobile lifestyle. The proposed project would appeal to nearby residents as well as those in the City at-large. Located to the east of the Goldenwest Transit Center, the development would promote the use of transit services as an alternative mode of travel in addition to walking and biking rather than relying on the automobile as the primary mode of transportation.

The proposed project incorporates architectural and design principles to provide a pedestrian-oriented scale and ensure maximum design compatibility with existing and proposed developments within the surrounding area. Structures on the project site utilize high quality exterior materials, articulated building volumes, and building composition through the use of building recesses and awnings. The project complies with the development standards and design guidelines of the BECSP, which ensure that form, height, and treatment convey an overall high level of quality. The retail structure is sited close to streets with a retail entry fronting onto the public sidewalk.

The project is designed to minimize consumption of natural resources through implementation of sustainability features such as recycled corrugated steel shipping containers as building materials, drought-tolerant landscaping, efficient lighting and plumbing fixtures, skylights, and waste recycling facilities. The project would comply with the BECSP and other City codes to reduce water consumption and stormwater runoff. A preliminary Water Quality Management Plan (WQMP) has been reviewed by Public Works staff. A final WQMP is required to be submitted for review and approval prior to grading permit issuance. Finally, the project will incorporate sustainable site development strategies, utilize water savings features, and emphasize recycling of resources and materials through design features and community policies.

Zoning Compliance:

The project is located within the Beach and Edinger Corridors Specific Plan and complies with the requirements of the Town Center Neighborhood segment except for the requested conditional use permits

and variances. The table below provides an overview of the project's conformance to the significant development standards of the BECSP. In addition, a list of City Code Requirements of the applicable provisions of the BECSP and the Huntington Beach Zoning and Subdivision Ordinance (HBZSO) and Municipal Code has been provided to the applicant and attached to this report (Attachment No. 4) for informational purposes only.

| Provision | Town Center - Neighborhood | Proposed Project |
|--|--|--|
| 2.2 Use Regulations | retail and civic & cultural permitted | skate park; skate shop |
| 2.2.2 Special Retail Configuration | n/a | n/a |
| 2.2.3 Affordable Housing | n/a | n/a |
| 2.3.1 & 2.3.2 Height & Special Building Height | 2 stories min.; 6 stories max. | 2 stories |
| 2.3.3 Building Length | 300' max. | 47' |
| 2.3.4 Special Building Length | n/a | n/a |
| 2.3.5 Building Massing (Volume Proportions) | 1L:3H to 3L:1H (ratio of length to height) | 1L:3H to 3L:1H |
| 2.4.1 Building Orientation to Streets | orientation to street or open space required | building is oriented to street |
| 2.4.2 Private Frontage | shopfronts, arcade, forecourt, grand portico, common lobby, stoop, terraced, flush permitted | shopfront provided |
| 2.4.3 Front Setback | 0' min./15' max. | 4'-5' |
| 2.4.4 Side Yard Setback | 10' w/living space windows; 0' w/out living space windows | n/a |
| 2.4.5 Rear Setback | 10' min. | 879' on north side |
| 2.4.6 Alley Setback | 5' min. | n/a |
| 2.4.7 Frontage Coverage | 90% min. | 47%; Variance requested |
| 2.4.8 Space Between Buildings | 20' min. | n/a |
| 2.4.9 Build to Corner | Required | n/a |
| 2.5.1 Improvements to Existing Streets | Center – Neighborhood Street required (sidewalk, planting strip, trees, lights) | Center – Neighborhood Street 2 lights required; 1 light provided; Variance requested |
| 2.5.2 Proposed streets | n/a | n/a |
| 2.5.3 Max. Block Size | 2,400 linear feet max. | n/a |
| 2.5.4 Street Connectivity | n/a | n/a |
| 2.5.5 East-West Street Connection | n/a | n/a |
| 2.5.6 Residential Transition Boundary | n/a | n/a |
| 2.5.7 Street types – New Street Design | n/a | n/a |
| 2.6.1 Provision of Public Open Space | n/a | n/a |
| 2.6.2 Special Public Open Space | n/a | n/a |
| 2.6.3 Provision of Private Open Space | n/a | n/a |
| 2.6.4 Public Open Space Types | n/a | n/a |
| 2.6.5 Private Open Space Types | n/a | n/a |

| Provision | Town Center - Neighborhood | Proposed Project |
|------------------------------------|--|---|
| 2.6.6 Stormwater Management | Required | provided – WQMP |
| 2.6.7 Stormwater BMP Types | Required | provided – WQMP |
| 2.6.8 Open Space Landscaping | required | n/a |
| 2.6.9 Setback Area Landscape Types | interior block setback areas— groundcover required for side and rear yards | complies; groundcover provided, moderate screening at interior perimeters |
| 2.7.1 Provision of Parking | civic & cultural – 3 min./4 max. per 1,000 sq. ft.; specialty foods/goods – 3 min./4 max. per 1,000 sq. ft.; Total Spaces Required = 90 250' max. distance for off-site parking for joint-use parking during special events (HBZSO 231.06) | 26 spaces proposed (normal operations) CUP requested for reduction in on-site parking CUP requested for off-site parking at the Sports Complex Variance requested for 2.8 miles distance |
| 2.7.2 Parking Types | surface lot – rear; wrapped ground level, wrapped all levels, partially submerged, underground structure permitted | surface lot – rear |
| 2.7.3 General Parking Requirements | 24' driveway width max. 5' driveway setback min. for driveway from adjoining properties 3' wide planter around parking facilities (HBZSO 232.08.C.2) | 26' driveway width; Variance requested 2' driveway setback; Variance requested 0' planter along east property line; Variance requested |
| 2.8.1 Façade Height | top/base required | Provided |

Urban Design Guidelines Conformance:

The project is required to comply with the architectural regulations and guidelines of the BECSP. The project proposes a contemporary architectural style, providing primary building volumes, building entries and architectural base treatments that are required by the BECSP development code. The contemporary architectural style is articulated and accentuated with different building materials, façade treatments, and utilization of geometric forms. Staying true to the contemporary architectural style, the project design employs a palette of building materials in unconventional ways for aesthetic purposes. The project includes the use of recycled corrugated steel shipping containers, concrete, and glass to create the building form. The proposed buildings are designed with flat roofs and utilize strong saturated building colors in order to be consistent with the features of contemporary styles. The project's architecture is reflective of the intent of the Town Center – Neighborhood segment in that the area is envisioned to have buildings built close to the sidewalks with entrances facing the street to encourage more pedestrian-oriented activities along the street and bring more vitality to the urban neighborhood.

Environmental Status:

Environmental Impact Report (EIR) No. 10-009 analyzed the potential environmental impacts of a public skate park and associated legislative amendments. EIR No. 10-009 determined that the majority of issue areas would have no impacts, less than significant impacts, or could be mitigated to less than significant level with incorporation of mitigation measures. On March 27, 2012, the Planning Commission certified EIR No. 10-009 as adequate and complete in accordance with CEQA requirements. The Planning Commission also adopted CEQA Findings of Fact with a Statement of Overriding Considerations with the approval of General Plan Amendment No. 11-002 and Zoning Text Amendment No. 11-002.

There are two project-specific significant adverse environmental impacts anticipated from the proposed project that cannot be completely eliminated through mitigation measures. The significant adverse environmental impacts are as follows:

- **Land Use Planning**
 - > **Project Specific**—The proposed project would result in conflicts with the Southern California Association of Governments Regional Housing Needs Assessment for 2006-2014 and the applicable goals and policies of the Huntington Beach General Plan.
- **Noise**
 - > **Project Specific**—The proposed project would result in temporary noise increases associated with periodic special events on-site, during which noise levels would exceed established thresholds at nearby sensitive receptors.

Although the project results in adverse impacts to the environment that cannot be mitigated or avoided, the Planning Commission may still approve the project if a Statement of Overriding Considerations finding that the economic and social benefits of the project outweigh its potentially adverse environmental impacts is adopted. CEQA requires decision makers to balance the benefits of the proposed project against its unavoidable environmental risks in determining whether to approve the project. If the benefits of a proposed project outweigh the unavoidable adverse environmental effects, the City may consider the adverse environmental effects acceptable. The adverse impacts are unavoidable because it has been determined that no feasible mitigation is available at this time.

Approval of the project facilitates the development of a public skate park that is consistent with the BECSP and General Plan. In addition, the project would have the following benefits:

- The proposed project would provide a new state-of-the-art skate park and associated retail/concession use that would be located in close proximity to public transit facilities and residential communities.
- The project would provide additional year-round, fee-free recreational opportunities within the City through provision of a state-of-the-art skate park on land owned by the City.
- The project would increase the City's tax revenue through sales tax from retail sales and periodic special events.
- The project would improve the visual quality and character of the project site through implementation of the design standards contained in the BECSP, including architectural features, lighting, signage, and landscaping.

- The project would minimize consumption of natural resources through implementation of sustainability features such as drought-tolerant landscaping, efficient lighting and plumbing fixtures, skylights, and waste recycling facilities.

Because implementation of the proposed project would create significant unavoidable impacts in the areas of Land Use/Planning and Noise, a Statement of Overriding Considerations is required to be adopted prior to action on the project.

Environmental Board:

The City's Environmental Board reviewed EIR No. 10-009 and provided a comment letter during the DEIR process. In summary, the Environmental Board stated concerns regarding water run-off impacts, noise and lighting impacts, and the work on high voltage power lines nearby. The Board provided suggestions for project design related to landscaping in the use of specific trees, shrubs, and groundcover. The Board also encouraged the use of sustainable development practices to increase the project's energy efficiency. As noted above, EIR No. 10-009 has already been certified by the Planning Commission.

Coastal Status: Not applicable.

Redevelopment Status: Not applicable.

Design Review Board: Not applicable.

Subdivision Committee: Not applicable.

Other Departments Concerns and Requirements:

The Departments of Planning and Building, Community Services, Economic Development, Fire, and Public Works have reviewed the proposed project and provided comments and recommendations. Any recommended conditions of approval are incorporated into the suggested conditions provided in Attachment No. 1.

Public Notification:

Legal notice was published in the Huntington Beach Independent on November 15, 2012, and notices were sent to property owners of record and occupants within a 1,000 ft. radius of the project site, interested parties, and individuals/organizations requesting notification (Planning and Building Department's Notification Matrix), applicant, and interested parties. As of November 20, 2012, no communication regarding the project has been received.

Application Processing Dates:

DATE OF COMPLETE APPLICATION:
Site Plan Review, Conditional Use Permit,
Variance: October 18, 2012

MANDATORY PROCESSING DATE(S):
Within 180 days of complete application:
April 18, 2013

Site Plan Review No. 2012-003, Conditional Use Permit No. 2012-009, and Variance No. 2012-004 were filed on April 19, 2012 and deemed complete on October 18, 2012.

ANALYSIS:

The primary issues identified below are compliance of the proposed project with the applicable Beach and Edinger Corridors Specific Plan requirements; land use compatibility of the proposed project with the surrounding properties; adequacy of and reduction to off-street parking requirements at the project site based on a parking demand analysis; adequacy of off-street parking requirements during special events at the Huntington Beach Sports Complex for joint use parking based on a parking demand analysis; and variances to permit deviations in the distance of joint use parking during special events, driveway width, driveway setback, parking area landscaping screening, frontage street lighting and frontage coverage.

Compliance with BECSP

The BECSP was adopted in April 2010 to enhance the overall economic performance, physical beauty and functionality of the Beach Boulevard and Edinger Avenue Corridors. The BECSP is intended to guide future development and initiate the transformation of the corridors from commercial strip, in many cases underutilized and underperforming, to a pattern of centers and segments with development standards and regulations that reflect the vision of a particular area. As previously mentioned, the project site is located within the Town Center - Neighborhood segment of the BECSP. The Town Center - Neighborhood segment includes the project site as well as the Goldenwest Transit Center and SCE property to the west and the Archstone HB Lofts property and the Boardwalk Mixed Use property to the south of the project site. The Town Center - Neighborhood segment is intended to form an urban neighborhood by providing the majority of residential units in the Edinger Avenue Corridor to support the Town Center - Core commercial uses and the regional commercial uses of Bella Terra and Edinger Avenue while benefiting from proximity to the Goldenwest Transit Center and Goldenwest College. This neighborhood would feature the City's widest range of contemporary housing types and a wide mixture of uses, all concentrated within walking distance of the Town Center Core's theater, shops, restaurants, cafes, nightlife, and amenities.

The proposed project is consistent with the intent of the Town Center - Neighborhood segment of the BECSP as stated above and overall objective to offer a wide mixture of uses to support the urban neighborhood. The proposed skate park would serve as a recreational amenity within walking distance for residents of key developments surrounding the project site (i.e. Village at Bella Terra, the Boardwalk, and Archstone HB Lofts mixed use projects). The presence of the proposed skate park would contribute to a higher quality of life by providing access to different yet complimentary activities and uses where people live, shop, and play in close proximity. In recognition of the unique potential of the project site, the BECSP development code requires standards and regulations that, when implemented, achieve the objectives intended for the Town Center - Neighborhood. As discussed under the Zoning Conformance section of this report, the project is in substantial compliance with the BECSP development code. The proposed project complies with code requirements regarding building height, building length, building orientation, building setbacks, building massing, parking types, and setback area landscaping types. A Variance is being processed concurrently to allow deviations in the distance of joint use parking during special events, driveway width, driveway setback, parking lot landscaping screening, frontage street lighting, and frontage coverage as discussed below.

Staff is recommending modifications to bring the design of the building into greater conformance with the Beach and Edinger Corridors Specific Plan development requirements. The modifications pertain to the street facing building façade in terms of primary volume, display windows for the ground level retail, horizontal articulation of the base and top elements, and wall composition. The staff recommended modifications are listed below:

- Require the primary volume of the south facing building façade to incorporate a major offset, notch, and façade composition change.
- Require the ground level retail shopfront of the south facing building façade to contain a minimum of 70% clear-glass display windows framed within storefront pilasters and base.
- Require the metal panel base of the south facing building façade to be provided for the entire building façade facing the street.
- Require a substantial horizontal articulation of the south facing building façade to be provided at the top of the uppermost floor of the façade.
- Require the wall composition of the south facing building façade to contain a minimum of twenty (20) percent and a maximum of sixty (60) percent glazing.

With the requested variances and modifications recommended by staff, the proposed project would comply with the BECSP.

Land Use Compatibility

In the immediate vicinity of the project site, there is an existing and anticipated mix of land uses such as commercial, entertainment, and residential uses (Bella Terra Mall, Costco, Seawind Village Apartments, single family neighborhood north of McFadden Avenue), an educational use (Golden West College), a transit hub (Golden West Transportation Center), and mixed-use developments (Old World Village, Archstone HB Lofts and Village at Bella Terra approved projects). The proposed skate park would complement a diversity of land uses, particularly along the Center Avenue corridor, and would serve as a recreational focal point for the surrounding community. As a free-of-charge recreational amenity, the proposed skate park would benefit the residents surrounding the project site and those in the City at-large. The proposed recreational facility, along with the mix of existing and anticipated land uses in the vicinity, would create a dynamic environment where people can live, work, and play within walking distance. Not only would the proposed project be compatible with the surrounding land uses, it would contribute to the synergistic mix of uses and people in creating a vibrant urban atmosphere and staff finds the proposed project compatible with surrounding uses.

Parking Requirements at the Project Site

The applicant is requesting the allowance of 26 parking spaces in lieu of the minimum required 90 parking spaces (64 space reduction) in conjunction with a 14,500 sq. ft. skate plaza, an 11,800 sq. ft. skate bowl, and a 3,500 sq. ft. skate shop. The applicant submitted a Parking Demand Analysis, prepared by Translutions, Inc. (September 2012) to substantiate the reduction in the parking requirement (Attachment No. 5). An analysis was provided between the off-street parking requirements of the Beach and Edinger Corridors Specific Plan and two parking surveys of similar skate parks (Etnies Skate Park in Lake Forest and Carmel Valley Skate Park in San Diego). The study concludes that 14 parking spaces would be required for uses proposed for the skate park. However, the project is proposing to provide 26 parking spaces. Based on the results of the study, there would be a surplus of 12 parking spaces onsite.

Staff supports the parking reduction because adequate on-site parking can be accommodated for the proposed skate park based on the Parking Demand Analysis prepared by Translutions, Inc. The study relied on two parking surveys of similar skate parks. The study determined that the proposed skate park would require 14 parking spaces. The parking for the skate park will not result in insufficient parking capacity. In addition, the site layout of the proposed development is designed to maximize the use of the site. Therefore, the parking reduction is not anticipated to result in a parking deficiency during normal daily operations at the project site and staff supports the request.

Parking Requirements during Special Events at the Sports Complex

The applicant is requesting the allowance of required parking during special events to be accommodated at the Huntington Beach Sports Complex, which is located approximately 2.8 miles from the project site. The Sports Complex has a total of 850 parking spaces. The proposed skate park would host up to 15 special event days throughout the year, expected to draw between 300 to 2,500 spectators. During these events, visitors would be directed to park their vehicles at the Huntington Beach Sports Complex and be transported to and from the skate park via shuttle buses.

The applicant submitted the Special Events Parking Demand Analysis, prepared by Translutions, Inc. (September 2012) to determine the potential parking demand during special events (Attachment No. 6). An analysis was conducted based on discussions with the City of Lake Forest staff at the Etnies Skate Park facility and the forecasted parking rates for other large congregation areas as determined by Institute of Transportation Engineers (ITE) *Parking Generation Manual*. The study concludes that the parking demand is anticipated to be 125 parking spaces for small events of up to 500 attendees and 625 parking spaces for large events of up to 2,500 attendees. Since there are 850 available parking spaces at the Sports Complex, there would be a surplus of 725 parking spaces during small events and 125 parking spaces during large events.

Staff supports the allowance of special events parking at the Huntington Beach Sports Complex because adequate parking can be provided based on the Special Events Parking Demand Analysis prepared by Translutions, Inc. The study determined that the Sports Complex could accommodate the parking demand generated during the proposed skate park special events. Because of the number of available parking spaces at the Sports Complex, the generated special events parking is not anticipated to result in a parking deficiency and staff supports the request.

Variances

Because of the limitation of the lot configuration, the applicant is requesting deviations in distance of joint use parking during special events, driveway width, driveway setback, parking lot landscaping screening, frontage street lighting, and frontage coverage.

- Distance of Joint Use Parking During Special Events
According to HBZSO Section 231.06 (Joint Use Parking), the maximum distance between the use and the nearest point of the parking facility is required to be 250 ft. Due to the small size of the lot and the skate park's physical constraint to provide onsite parking for special events, the applicant is proposing to provide parking for special events at the Huntington Beach Sports Complex, which is located approximately 2.8 miles from the project site. During these events, visitors would be

directed to park their vehicles at the Huntington Beach Sports Complex and be transported to and from the skate park via shuttle buses. Up to six shuttle buses would be utilized to accommodate guest demand and shuttle drivers would follow a specified shuttle route on Gothard Street between Center Avenue and Talbert Avenue. Prior to scheduling special events, the project applicant would coordinate with the Community Services Department to ensure that no events are planned at the Sports Complex at the same time.

While adequate parking for special events is anticipated to be provided at the Huntington Beach Sports Complex, special events have the potential to create parking impacts for adjacent neighbors. Therefore, a Parking Management Plan is required through Mitigation Measure MM 4.G-1 address the potential for overflow parking during special events. Prior to the first special event, a Parking Management Plan would be submitted to the Public Works Department for review and approval.

MM 4.G-1 Prior to special events, the skate park operator, in consultation with the City of Huntington Beach and the adjacent property owners, shall implement measures to the extent feasible to minimize the potential for off-site parking impacts to neighboring uses. This shall include obtaining all necessary permissions or permits as needed. These measures can include, but are not limited to, the following:

- *Provide access management for the staging area, including personnel to guide/direct visitors to appropriate parking areas;*
- *Provide management techniques for use of the overflow parking, including the use of valet parking in a portion of the remote lot (Huntington Beach Sports Complex);*
- *Provide permit parking and supplemental patrol for residential neighborhoods adjacent to the project site at no cost to residents; and*
- *Provide signage to direct visitors to the remote lot and discourage visitors from parking in adjacent residential neighborhoods and the Bella Terra commercial area;*
- *Post "No Event Parking" signs or similar at entrances to adjacent lots and provide attendants or monitoring as requested or needed; and*
- *Meet with adjacent property owners as needed to determine mitigation measure effectiveness and amend programs to address unforeseen impacts, as feasible.*

With the proposed shuttle service and implementation of the Parking Management Plan, staff supports this request.

- Driveway Width

City Specification No. 401 (Minimum Standards for Fire Apparatus Access) requires that driveways shall not be less than 27 ft. wide. However, Fire Department staff has indicated that a minimum 26 ft. wide driveway would be sufficient based on the layout of the site for the proposed project. The BECSP allows a maximum width of 24 ft. for a two-lane driveway to facilitate a more vital and attractive pedestrian-oriented environment. In order to comply with City Specification No. 401, the applicant is requesting to provide a 26 ft. wide driveway. Staff supports the deviation in driveway width to conform with the life and safety requirements of the Fire Department.

- Driveway Setback

The BECSP requires driveways to be setback a minimum of five ft. from adjoining properties. Because of the limited width of the project site and the requirements of City Specification No. 401, the applicant is requesting to deviate from the required minimum driveway setback. Fire access road turns and corners are required to be designed with a minimum inner radius of 17 ft. and a minimum outer radius of 45 ft. With the lot width of 100 ft. and an approximately seven ft. wide railroad easement, there is only approximately three ft. remaining for a setback. Staff supports the reduction in the driveway setback because of the limitation in lot area to accommodate the required driveway setback.

- Parking Area Landscaping Screening

The HBZSO requires parking facilities to have a minimum three ft. wide planter for perimeter landscaping area. Because of the limited width of the project site and City Specification No. 401, the applicant is requesting to deviate from the required minimum landscaped planter width. The 90 ft. wide vehicle turn-around area and the approximately seven ft. wide railroad easement leave an approximately three ft. area for landscaping areas. Zero landscaping is proposed for the vehicle turn-around area on the east side of the property and a two ft. wide planter is proposed for the vehicle turn-around area on west side of the property. Staff supports the reduction in the perimeter landscaping area because of the limitation of the lot area to accommodate the required landscaping area.

- Frontage Street Lighting

The BECSP requires the Neighborhood Streets design to be installed for the pedestrian zone to include pedestrian-scale decorative street lighting at a maximum spacing for 90 ft. on-center. Based on the lot width of 100 ft., two street lights are required to be provided. Due to the approximately 49 ft. of frontage available for lighting installation (excluding the 20 ft. transit reserve area, 26 ft. driveway, and 5 ft. driveway setback from the adjacent Southern California Edison property), the applicant is requesting to provide one street light in lieu of two required lights. There is an existing street light located 15 ft. west of the eastern property line. Staff supports the reduction in the number of street lights because of the limitation of street frontage to provide the required street lights.

- Frontage Coverage

The BECSP requires the minimum percentage of the length of the primary building façade (frontage coverage) to be 90 percent in the Town Center – Neighborhood segment. Due to the narrow and long lot configuration (100 ft. by 1,190 ft.) and the buildable street frontage (88 ft.), the applicant is requesting to deviate from the required minimum frontage coverage for the building façade. The site configuration requires the project to be designed to have activities and improvements at a considerable distance from the street. Because the buildable street frontage is only 88 ft., the proposed 47 percent frontage coverage allows for visual and physical access to the site activities and for the functional use of the property in providing the 26 ft. driveway and 5 ft. driveway setback from the adjacent Southern California Edison property. Staff supports the reduction in the frontage coverage because of the limitation of the lot configuration to provide the required frontage coverage.

SUMMARY:

Staff recommends the Planning Commission approve Site Plan Review No. 2012-003, Conditional Use Permit No. 2012-009, and Variance No. 2012-004 with modifications based upon the following:

- Consistent with good zoning practice and implements the goals the BECSP while conforming to the standards and regulations set forth in the development code, except for the requested variances.
- Facilitates the development of a public skate park to provide additional recreational opportunities for the community.
- Improves the City's fiscal viability by stimulating tourism and increasing tax revenues in conjunction with the skate park special events.
- The proposed project will be compatible with surrounding existing and anticipated land uses.
- Complements a diversity of uses within the Beach and Edinger Corridors Specific Plan (BECSP) area, particularly along the Center Avenue corridor.
- The project furthers goals, objectives, and policies of the General Plan.
- The project site has physical hardships, resulting from the shape of the site, that warrant approval of requested deviations in the distance of joint use parking during special events, driveway width, driveway setback, parking lot landscaping screening, frontage street lighting, and frontage coverage.

ATTACHMENTS:

1. Suggested Findings and Conditions of Approval for Site Plan Review No. 2012-003, Conditional Use Permit No. 2012-009, and Variance No. 2012-004
2. Site Plans, Floor Plans, Elevations, and other site plan exhibits dated September 27, 2012
3. Project Narrative dated September 27, 2012
4. Code Requirements Letter dated October 18, 2012 (for informational purposes only)
5. Center Avenue Skate Park Parking Demand Analysis dated September 27, 2012
6. Special Events Parking Demand Analysis dated September 27, 2012
7. CEQA Findings of Fact with Statement of Overriding Considerations – EIR No. 2010-009
8. Mitigation Monitoring and Reporting Program – EIR No. 2010-009
9. Environmental Impact Report No. 2010-009 – **Not Attached – (Available at City Hall – 3rd Floor Planning and Building Department and on the City's website)**

SH:JJ:TN

ATTACHMENT NO. 1

SUGGESTED FINDINGS AND CONDITIONS OF APPROVAL

SITE PLAN REVIEW NO. 2012-003
CONDITIONAL USE PERMIT NO. 2012-009
VARIANCE NO. 2012-004

SUGGESTED FINDINGS FOR APPROVAL – SITE PLAN REVIEW NO. 2012-003:

1. Site Plan Review No. 12-003 for the construction of a skate park consisting of a 14,500 sq. ft. skate plaza, an 11,800 sq. ft. skate bowl, a 3,500 sq. ft. skate shop, a 185 sq. ft. restroom structure, a 150 sq. ft. skate park entrance kiosk, 26 parking spaces and associated improvements will not be detrimental to the general welfare of persons working or residing in the vicinity or detrimental to the value of the property and improvements in the neighborhood. The skate park development, with the recommended conditions of approval, incorporates architectural and design elements that provide maximum compatibility of design with the existing and anticipated development surrounding the project site, promotes pedestrian accessibility, and promotes the image of the Huntington Beach envisioned within the Beach and Edinger Corridors Specific Plan (BECSP). Structures on the project site are one and two stories in height and feature enhanced building materials and colors, building recesses and façade offsets, and variation in massing composition. The project's conformance to the Beach and Edinger Corridors Specific Plan (BECSP) further ensures that the form, height, and architectural design convey an overall high level of quality.
2. The project will not adversely affect the Circulation Plan of the BECSP. The project will provide public right-of-way improvements along Center Avenue pursuant to the BECSP development code. The improvements to Center Avenue establish a public realm that focuses on pedestrians rather than vehicles and provide a sufficient landscape buffer to separate pedestrians from the Center Avenue vehicular thoroughfare. The public frontage treatments incorporate minimum six-foot wide sidewalks, street trees and planting areas. No additional street improvements are required to improve capacity/efficiency on intersection operations; however, the project will pay fees commensurate with the project's contribution of traffic on the area-wide roadway system.
3. The proposed skate park development will comply with the applicable provisions of the Beach and Edinger Corridors Specific Plan development code as specified in the Town Center – Neighborhood segment. In addition, the project meets all applicable provisions of Titles 20-25 of the Huntington Beach Zoning and Subdivision Ordinance. The proposed project meets code requirements in terms of building height, length, orientation, massing and volume, setbacks, parking types, and setback area landscaping types. The project also ensures that the form and architectural design convey an overall high level of quality materials consistent with the vision of the Specific Plan. A Variance is being processed concurrently to allow deviations in the distance of joint use parking during special events, driveway width, driveway setback, parking lot landscaping screening, frontage street lighting, and frontage coverage.
4. The granting of the site plan review will not adversely affect the General Plan. It is consistent with the Land Use Element designation of Mixed Use–Specific Plan Overlay–Design Overlay on the

subject property. In addition, it is consistent with the following goals, policies, and objectives of the General Plan:

A. Air Quality Element

Goal AQ 1: Improve regional air quality by a) decreasing reliance on single occupancy vehicular trips, b) increasing efficiency of transit, c) shortening vehicle trips through a more efficient jobs-housing balance and a more efficient land use pattern, and d) increasing energy efficiency.

Policy AQ 1.10.1: Continue to require the utilization and installation of energy conservation features in all new construction.

B. Circulation Element

Objective CE 2.3: Ensure that the location, intensity and timing of new development is consistent with the provision of adequate transportation infrastructure and standards as defined in the Land Use Element.

Goal CE 2.3.4: Require new development mitigate its impact on City streets, including but not limited to, pedestrian, bicycle, and vehicular conflicts, to maintain adequate levels of service.

Policy CE 6.1.6: Maintain existing pedestrian facilities and require new development to provide pedestrian walkways and bicycle routes between developments, schools, and public facilities.

C. Land Use Element

Goal LU 2: Ensure that development is adequately served by transportation infrastructure, utility infrastructure, and public services.

Goal LU 4: Achieve and maintain high quality architecture, landscape, and public open spaces in the City.

Goal LU 7: Achieve a diversity of land uses that sustain the City's economic viability, while maintaining the City's environmental resources and scale and character.

Objective LU 7.1: Accommodate the development of a balance of land uses that (a) provides for the housing, commercial, employment, educational, cultural, entertainment, and recreation needs of existing and future residents, (b) provides employment opportunities for residents of the City and surrounding subregion, (c) captures visitor and tourist activity, and (d) provides open space and aesthetic "relief" from urban development.

D. Recreation and Community Services Element

Goal RCS 1: Enrich the quality of life for all citizens of Huntington Beach by providing constructive and creative leisure opportunities.

Objective RCS1.1: Encourage recreational opportunities unique to Huntington Beach which will enhance visitation and economic development.

E. Urban Design Element

Goal UD 1: Enhance the visual image of the City of Huntington Beach.

Objective UD 1.3: Strengthen the visual character of the City's street hierarchy in order to clarify the City's structure and improve Citywide identity.

Policy UD 1.1.3: Require a consistent design theme and/or landscape design character along the community's corridors that reflects the unique qualities of each district. Ensure that streetscape standards for the major commercial corridors, the residential corridors, and primary and secondary image corridors provide each corridor with its own identity while promoting visual continuity throughout the City.

The project would provide a new state-of-the art skate park and associated retail use that enriches the quality of life for residents of the City, enhances economic development, allows for more efficient use of land resources, and achieves high quality public open spaces in the City. The area has a variety of complementary uses that are critical to any vibrant community such as regional commercial and entertainment uses, employment centers, a transit hub, a community college. Because of its location, the site would be appropriate in accommodating an infill development that serves as a recreational amenity to support the residential communities (i.e. Village at Bella Terra, the Boardwalk and Archstone HB Lofts mixed use projects) surrounding the project site. The project would increase the City's tax revenue through sales tax from retail sales and periodic special events. In doing so, multiple sustainable development principles are achieved, resulting in the recreational and economic well-being of the area. The project provides access to recreational opportunities for residents seeking to be within walking distance of work, entertainment, and recreation, thus reducing dependency on their automobile and having access to multiple amenities for an active and mobile lifestyle. The proposed project would appeal to nearby residents as well as those in the City at-large. Located to the east of the Goldenwest Transit Center, the development would promote the use of transit services as an alternative mode of travel in addition to walking and biking rather than relying on the automobile as the primary mode of transportation.

The proposed project incorporates architectural and design principles to provide a pedestrian-oriented scale and ensure maximum design compatibility with existing and proposed developments within the surrounding area. Structures on the project site utilize high quality exterior materials, articulated building volumes, and building composition through the use of building recesses and awnings. The project complies with the development standards and design guidelines of the BECSP, which ensure that form, height, and treatment convey an overall high level of quality. The retail structure is sited close to streets with a retail entry fronting onto the public sidewalk.

The project is designed to minimize consumption of natural resources through implementation of sustainability features such as recycled corrugated steel shipping containers as building materials, drought-tolerant landscaping, efficient lighting and plumbing fixtures, skylights, and waste recycling facilities. The project would comply with the BECSP and other City codes to reduce water

consumption and stormwater runoff. A preliminary Water Quality Management Plan (WQMP) has been reviewed by Public Works staff. A final WQMP is required to be submitted for review and approval prior to grading permit issuance. Finally, the project will incorporate sustainable site development strategies, utilize water savings features, and emphasize recycling of resources and materials through design features and community policies.

SUGGESTED FINDINGS FOR APPROVAL - CONDITIONAL USE PERMIT NO. 2012-009:

1. Conditional Use Permit No. 2012-009 to permit a parking reduction of 64 parking spaces in conjunction with a 14,500 sq. ft. skate plaza, an 11,800 sq. ft. skate bowl, and a 3,500 sq. ft. skate shop and to permit required parking during special events to be accommodated off-site at the Huntington Beach Sports Complex (2.8 miles from the project site) are based on a parking demand analyses, prepared by Translutions, Inc. and Joseph Jimenez, a state-registered traffic engineer (September 2012). An analysis for the on-site reduction of required parking was provided between the City's off-street parking requirements of the Beach and Edinger Corridors Specific Plan and two parking surveys of similar skate parks (Etnies Skate Park in Lake Forest and Carmel Valley Skate Park in San Diego). The study concludes that 14 parking spaces would be required for uses proposed for the skate park. However, the project is proposing to provide 26 parking spaces. Based on the study, there would be a surplus of 12 parking spaces onsite. The study determined that parking for the skate park will not result in insufficient parking capacity. An analysis for the off-site parking at the Huntington Beach Sports Complex was conducted based on discussions with the City of Lake Forest staff at the Etnies Skate Park facility and the forecasted parking rates for other large congregation areas as determined by Institute of Transportation Engineers (ITE) Parking Generation Manual. The study concludes that the parking demand is anticipated to be 125 parking spaces for small events of up to 500 attendees and 625 parking spaces for large events of up to 2,500 attendees. Since there are 850 available parking spaces at the Sports Complex, there would be a surplus of 725 parking spaces during small events and 125 parking spaces during large events. The study determined that the Sports Complex could accommodate the parking demand generated during the proposed skate park special events.
2. The proposed skate park will generate a demand of 14 parking spaces based on a parking demand analysis prepared by Translutions, Inc. and Joseph Jimenez. The study relied on two parking surveys of similar skate parks (Etnies Skate Park in Lake Forest and Carmel Valley Skate Park in San Diego). The study determined that the proposed skate park will not result in a parking deficiency at the project site. The special events parking will generate a demand of 125 parking spaces for small events of up to 500 attendees and 625 parking spaces for large events of up to 2,500 attendees based on a parking demand analysis prepared by Translutions, Inc. and Joseph Jimenez. The study was conducted based on discussions with the City of Lake Forest staff at the Etnies Skate Park facility and the forecasted parking rates for other large congregation areas as determined by ITE Parking Generation Manual. The study determined that the Sports Complex could accommodate the parking demand generated during the proposed skate park special events.
3. A Transportation Demand Management Plan was prepared by Cristal Garcia of SITE Design Group, Inc. (November 2012) for the proposed skate park. The Plan incorporates transportation demand management measures such as bicycle racks, transit information. The Plan integrates the

transportation demand management strategies required by HBZSO Section 230.36 and has been approved by the Director of Planning and Building.

SUGGESTED FINDINGS FOR APPROVAL – VARIANCE NO. 2012-004:

1. The granting of Variance No. 2012-004 to allow a 2.8 mile-distance for joint use parking during special events, a 26 ft.-wide driveway, a 2-foot setback for driveways, zero landscaping screening for parking area, one light standard for frontage street lighting, and 47% frontage coverage for the primary building facade will not constitute a grant of special privilege inconsistent with limitations upon other properties in the vicinity and under an identical zone classification. Due to the narrow lot configuration (100 ft. by 1,190 ft.), the project would not be able to comply with the required development standards. The available buildable area is reduced further by the railroad easement and the transit reserve area. In addition, life and safety requirements from the Fire Department necessitate deviations in development standards. Approval of the requested deviations will not constitute a grant of special privilege as the variance will allow the development of a skate park to provide a functional use of the property and offer additional recreational opportunities to complement other uses in the vicinity.
2. Because of special circumstances applicable to the subject property, including size, shape, topography, location or surroundings, the strict application of the zoning ordinance is found to deprive the subject property of privileges enjoyed by other properties in the vicinity and under identical zone classification. The site has a narrow and long configuration with a width of 100 ft. and a length of 1,190 ft. Because of its size and shape, the property would not be able to provide the setbacks or comply with other development standards as compared to a larger site with a standard shape. Granting the deviations in setbacks, landscaping, and other standards allow the subject property to enjoy the same privileges as the other properties in the vicinity and under identical zone classification.
3. The granting of a variance is necessary to preserve the enjoyment of one or more substantial property rights. The requested variances are necessary to allow the proposed project to be developed with a skate park a public recreational amenity to serve the residential communities in close proximity to the subject site. Granting the requested variances will not affect the overall visual character and scale of the proposed development onsite in the context of the surrounding area. The subject site is able to enjoy the right to develop the site in accordance with the Town Center – Neighborhood segment of the Beach and Edinger Corridors Specific Plan.
4. The granting of the variance will not be materially detrimental to the public welfare or injurious to property in the same zone classification and is consistent with the General Plan. The development of a skate park will not be materially detrimental to the public welfare because the proposed buildings are designed to be consistent with the scale and character of the surrounding area. The proposed two-story and one-story structures would be consistent with the established development pattern in the vicinity. The granting of the variances will not adversely affect the General Plan. It is consistent with the Land Use Element designation of M-sp-d (Mixed Use-Specific Plan Overlay-Design Overlay) on the subject property.

SUGGESTED CONDITIONS OF APPROVAL – SITE PLAN REVIEW NO. 2012-003/CONDITIONAL USE PERMIT NO. 2012-009/VARIANCE NO. 2012-004:

1. The site plan, floor plans, and elevations received and dated September 27, 2012, shall be the conceptually approved design with following modifications:
 - a. The primary volume of the south facing building façade shall incorporate a major façade offset, notch, and façade composition change. **(BECSP Section 2.3.5(3))**
 - b. The ground level retail shopfront of the south facing building façade shall contain a minimum of 70% clear-glass display windows framed within storefront pilasters and base. **(BECSP Section 2.4.2(3))**
 - c. The metal panel base of the south facing building façade shall be provided for the entire building façade facing the street. **(BECSP Section 2.8.1(3)(a))**
 - d. A substantial horizontal articulation of the south facing building façade shall be provided at the top of the uppermost floor of the façade. The horizontal articulation shall be reviewed and approved by the Director of Planning and Building. **(BECSP Section 2.8.1(3)(b))**
 - e. The wall composition of the south facing building façade shall contain a minimum of twenty (20) percent and a maximum of sixty (60) percent glazing (percentage does not include parapet height). **(BECSP Section 2.8.2(1))**
2. The project shall comply with all mitigation measures adopted for the project in conjunction with Environmental Impact Report No. 10-009.
3. At least 14 days prior to any grading activity, the applicant/developer shall provide notice in writing to property owners of record and tenants of properties within a 500-foot radius of the project site as noticed for the public hearing. The notice shall include a general description of planned grading activities and an estimated timeline for commencement and completion of work and a contact person name with phone number. Prior to issuance of the grading permit, a copy of the notice and list of recipients shall be submitted to the Planning and Building Department.
4. Prior to submittal for building permits, the following shall be completed:
 - a. One set of project plans, revised pursuant to Condition of Approval No. 1, shall be submitted for review, approval and inclusion in the entitlement file, to the Planning Division.
 - b. Zoning entitlement conditions of approval, code requirements identified herein and code requirements identified in separately transmitted memorandum from the Departments of Fire and Public Works shall be printed verbatim on one of the first three pages of all the working drawing sets used for issuance of building permits (architectural, structural, electrical, mechanical and plumbing) and shall be referenced in the sheet index. The minimum font size utilized for printed text shall be 12 point.

- c. Submit three (3) copies of the site plan and floor plans and the processing fee to the Planning and Building Department for addressing purposes. The address assignment shall be reviewed and approved prior to submittal for building permits.
5. Prior to issuance of building permits, the following shall be completed:
 - a. An "Acceptance of Conditions" form shall be properly executed by the applicant and an authorized representative of the owner of the property, recorded with the County Recorder's Office, and returned to the Planning and Building Department for inclusion in the entitlement file. Conditions of approval shall remain in effect in the recorded form in perpetuity, except as modified or rescinded pursuant to the expressed written approval of the City of Huntington Beach.
 - b. Prior to connection of the subject site's sanitary sewer lateral to the existing sewer main in McFadden Avenue, the *Provision of Sewer Agreement* shall be executed between the City and the Midway City Sanitation District (MCSD). Copies of all related connection approvals and proof of payment of applicable MCSD fees shall be provided to the City Department of Public Works.
(PW)
6. The structure(s) cannot be occupied, the final building permit(s) cannot be approved, and utilities cannot be released, and issuance of a Certificate of Occupancy until the following has been completed:
 - a. Compliance with all conditions of approval specified herein are accomplished and verified by the Planning and Building Department.
 - b. All improvements must be completed in accordance with approved plans, except as provided for by conditions of approval.
 - c. All building spoils, such as unusable lumber, wire, pipe, and other surplus or unusable material, shall be disposed of at an off-site facility equipped to handle them.
 - d. A Certificate of Occupancy must be approved by the Planning Division and issued by the Building and Safety Division.
7. The skate park use shall comply with the following:
 - a. The hours of operation of the skate park shall be from 10:00 AM to 10:00 PM seven days a week.
 - b. The Transit Reserve Area for potential future development of a transit platform shall consist of a 20 foot wide by 300 foot long area as illustrated on the site plan and shall be clear of permanent structures.
 - c. The public address system may be used periodically during normal daily operations. During special events, amplified music and announcements from the public address system may continue through the duration of the event.

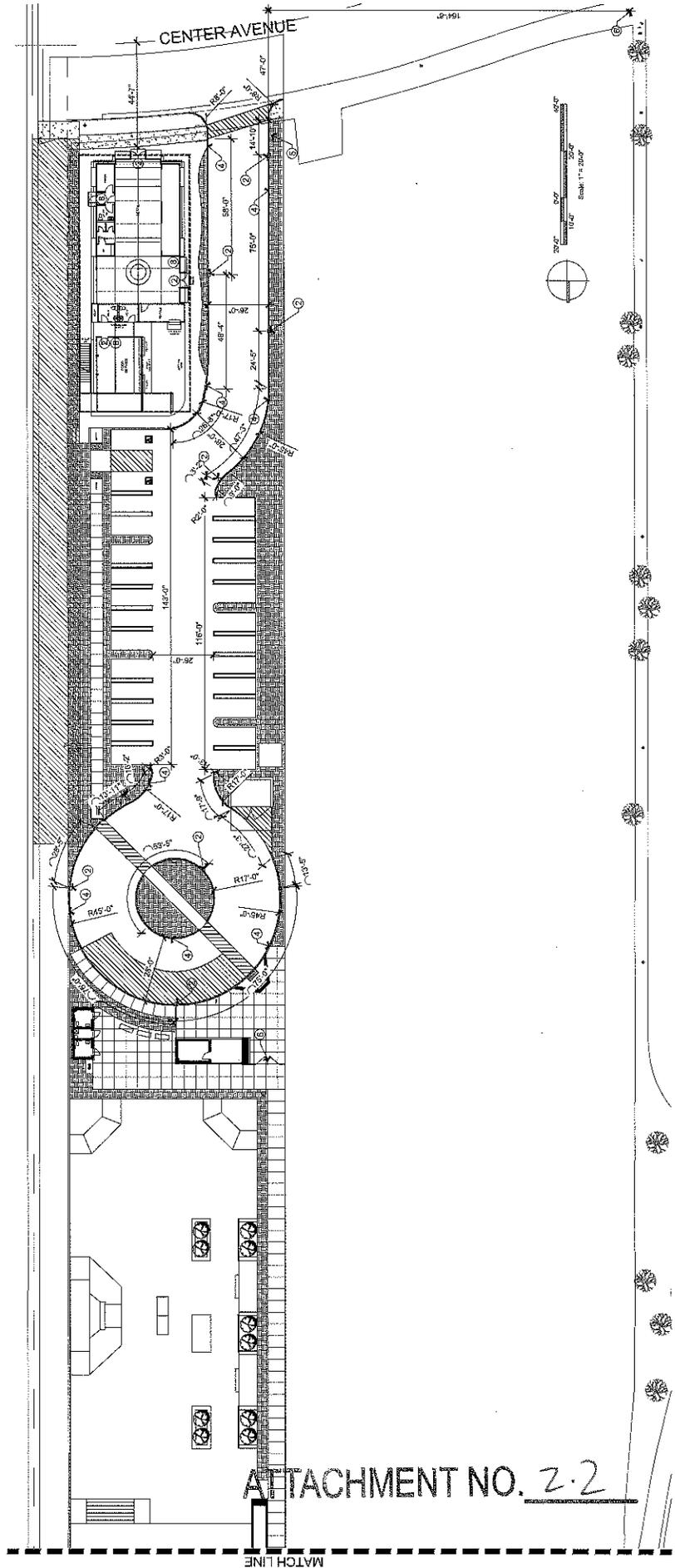
- d. The skate park shall remain free of charge and open to the public, with the exception of 15 event days per year.
 - e. Up to six shuttle buses shall be utilized to provide transportation from the Huntington Beach Sports Complex to the project site for skate park attendants during special events. The shuttle buses shall follow the specified shuttle route on Gothard Street between Center Avenue and Talbert Avenue.
8. Signage shall be reviewed under separate permits and applicable processing.
 9. The Development Services Departments (Building & Safety, Fire, Planning and Public Works) shall be responsible for ensuring compliance with all applicable code requirements and conditions of approval. The Director of Planning and Building may approve minor amendments to plans and/or conditions of approval as appropriate based on changed circumstances, new information or other relevant factors. Any proposed plan/project revisions shall be called out on the plan sets submitted for building permits. Permits shall not be issued until the Development Services Departments have reviewed and approved the proposed changes for conformance with the intent of the Planning Commission's action. If the proposed changes are of a substantial nature, an amendment to the original entitlement reviewed by the Planning Commission may be required pursuant to the provisions of HBZSO Section 241.18.
 10. The applicant and/or applicant's representative shall be responsible for ensuring the accuracy of all plans and information submitted to the City for review and approval.
 11. Site Plan Review No. 2012-003, Conditional Use Permit No. 2012-0009 and Variance No. 2012-004 shall become null and void unless exercised within two years of the date of final approval by the Planning Commission, or such extension of time as may be granted by the Director pursuant to a written request submitted to the Planning Department a minimum 30 days prior to the expiration date.
 12. Incorporation of sustainable or "green" building practices into the design of the proposed structures and associated site improvements is highly encouraged. Sustainable building practices may include (but are not limited to) those recommended by the U.S. Green Building Council's Leadership in Energy and Environmental Design (LEED) Program certification (<http://www.usgbc.org/DisplayPage.aspx?CategoryID=19>) or Build It Green's Green Building Guidelines and Rating Systems (<http://www.builditgreen.org/green-building-guidelines-rating/>).

INDEMNIFICATION AND HOLD HARMLESS CONDITION:

The owner of the property which is the subject of this project and the project applicant if different from the property owner, and each of their heirs, successors and assigns, shall defend, indemnify and hold harmless the City of Huntington Beach and its agents, officers, and employees from any claim, action or proceedings, liability cost, including attorney's fees and costs against the City or its agents, officers or employees, to attack, set aside, void or annul any approval of the City, including but not limited to any approval granted by the City Council, Planning Commission, or Design Review Board concerning this project. The City shall promptly notify the applicant of any claim, action or proceeding and should cooperate fully in the defense thereof.

PROPOSED ITEMS

- ① FIRE LANE SIGNS & POST PER CITY OF HUNTINGTON BEACH CITY SPECIFICATION NO. 415, SIGN #2
- ② FIRE LANE CURB PAINTING REFER TO DETAIL #1 ON SHEET SP-1.2
- ③ FIRE LANE MARKINGS PER CITY OF HUNTINGTON BEACH CITY SPECIFICATIONS NO. 415, OPTION 2. REFER TO DETAIL #2 ON SHEET SP-1.2
- ④ INSTALL FIRE LANE SIGNAGE PER DETAIL #3 ON SHEET SP-1.2
- ⑤ KNOX BOX MODEL 3284, FOR FIRE DEPT ACCESS. - REFER TO DETAIL #5 ON SHEET SP-1.0
- ⑥ CITY OF HUNTINGTON BEACH EXISTING FIRE HYDRANT LOCATION

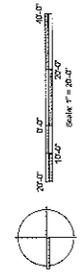
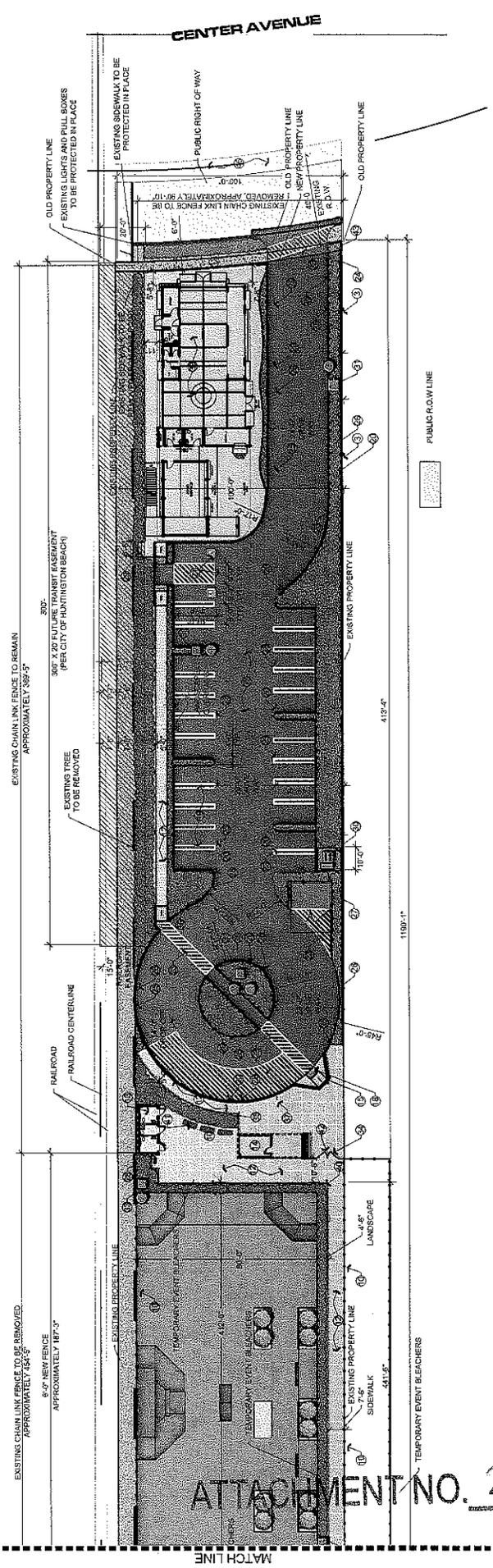
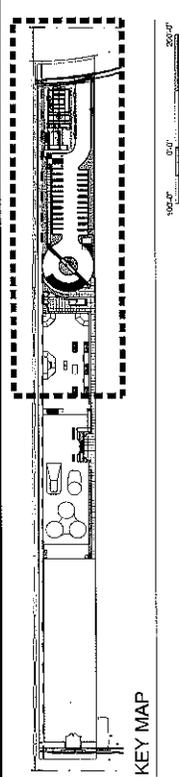


ATTACHMENT NO. 2.2

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|--|--|--|--|---|--|--|--|---|--|---|--|--|--|---------------------------------|--|---|--|-------------------------------|--|
| | | SKATE PARK CONSULTANT CA CALIFORNIA LANDSCAPE ARCHITECTURE & ENGINEERING, INC. 2777 Lakeside Avenue West, Suite 100 Berkeley, CA 94704 Phone: 925-835-2211 Fax: 925-835-2212 | | LANDSCAPE DESIGNER CA CALIFORNIA LANDSCAPE ARCHITECTURE & ENGINEERING, INC. 2777 Lakeside Avenue West, Suite 100 Berkeley, CA 94704 Phone: 925-835-2211 Fax: 925-835-2212 | | SKATE PARK CONSULTANT SUE BIRCH 2777 Lakeside Avenue West, Suite 100 Berkeley, CA 94704 Phone: 925-835-2211 Fax: 925-835-2212 | | ENGINEERING COMPANY RICK ENGINEERING COMPANY 1775 Howe Avenue, Suite 100 Berkeley, CA 94704 Phone: 925-835-2211 Fax: 925-835-2212 | | ELECTRICAL ENGINEER KENNETH HAN 401 E Street, Suite 200 San Diego, CA 92101 Phone: 619-744-0032 | | PROFESSIONAL SEAL BIRCH 2777 Lakeside Avenue West, Suite 100 Berkeley, CA 94704 Phone: 925-835-2211 Fax: 925-835-2212 | | REVISIONS No. DATE BY | | PROJECT ADDRESS: Located on the Enginer Corridor Between Center Ave. and McFadden Ave. east of Gothard St. | | SHEET NUMBER SP-0.3 | |
| VANS HUNTINGTON BEACH SKATE PARK + RETAIL Site Plan Submittal Fire Plan | | | | | | | | | | | | | | | | | | | |

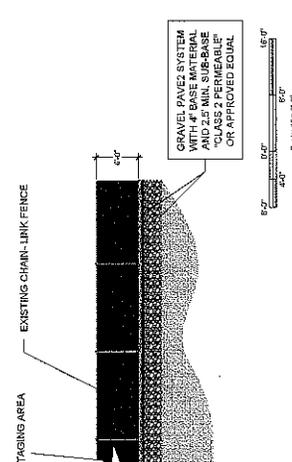
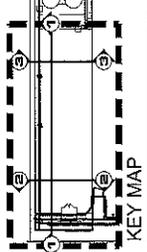
PROPOSED ITEMS

1. INSTALL STAGING AREA - REFER TO DETAIL 4 ON SHEET C.2.
2. CONSTRUCT DRIVEWAY APPROACH PER CITY OF HUNTINGTON BEACH STD DWG 211 (R-08)
3. CONSTRUCT PROPOSED 1" EDGE CURB - REFER TO DETAIL 1 ON SHEET C.2
4. STABILIZED DECOMPOSED GRANITE (CALIFORNIA GOLD) BY GAIL MATERIALS
5. GATE COLUMN - REFER TO DETAIL #1 ON SHEET SP-1.0
6. DOUBLE GATE - REFER TO DETAIL #4 ON SHEET SP-1.0
7. RETAINING WALL - REFER TO DETAIL #4 ON SHEET SP-1.1
8. 3'-6" GUARD RAIL - REFER TO DETAIL #7 ON SHEET SP-1.0
9. 3'-6" NEW FENCE - REFER TO DETAIL #6 ON SHEET SP-1.0
10. 6'-0" NEW FENCE - REFER TO DETAIL #5 ON SHEET SP-1.0
11. STAIRS WITH HANDRAILS - REFER TO TYPICAL DETAIL #7 ON SHEET SP-1.1
12. SIDEWALK / WALKWAY - REFER TO DETAIL #6 ON SHEET SP-1.1
13. RESTROOMS - REFER TO SHEETS A-1, A-2, A-3
14. ENTRY KIOSK - REFER TO SHEETS A-1, A-2, A-3
15. INSTALL CURB RAMP CASE A-TYPE 1 - REFER TO SHEET SP 1.1 DETAIL #2, #5
16. INSTALL CURB RAMP CASE B-TYPE 1 - REFER TO SHEET SP 1.1 DETAIL #3, #5, #7
17. INSTALL CURB RAMP CASE D-TYPE 1 - REFER TO SHEET SP 1.1 DETAIL #4, #6, #7
18. INSTALL TRUNCATED DOMES PER DETAIL 8 ON SHEET C.2
19. 24 PARKING STALLS & 2 HANDICAP ACCESSIBLE STALLS - REFER TO DETAIL #3, 13, 14, 15 ON SHEET C.2
20. INSTALL 6" CURB TYPE "B-2" - REFER TO SHEET SP 3.0 DETAIL #1
21. INSTALL ACCESSIBLE PARKING STRIPING PER DETAIL 13 & SA PARKING SYMBOL ON SHEET C.2
22. INSTALL ACCESSIBLE PARKING STRIPING PER DETAIL 14 ON SHEET C.2
23. INSTALL 4" STRIPING PER DETAIL 15 ON SHEET C.2
24. EXISTING CHAIN LINK FENCE TO BE REMOVED - APPROXIMATELY 336'-5"
25. EXISTING CHAIN LINK FENCE TO BE PROTECTED IN PLACE
26. EXISTING LIGHTS AND PULL BOXES TO BE PROTECTED IN PLACE
27. EXISTING SIDEWALK TO BE PROTECTED IN PLACE
28. PUBLIC ROW TO BE REPLACED BY PUBLIC WORKS, SEE SECTION #10 & #11 ON SHEET SP-0.7
29. INSTALL FIRE LAME SIGNAGE PER DETAIL #3 ON SHEET SP-1.2
30. FIRE LAME MARKINGS PER CITY OF HUNTINGTON BEACH CITY SPECIFICATION NO. 443, OPTION 2, REFER TO DETAIL 2 ON SHEET SP-1.2
31. FIRE LAME CURB PAINTING REFER TO DETAIL #1 ON SHEET SP 1.2
32. TRASH ENCLOSURE - REFER TO DETAIL #8 ON SHEET SP-0.8
33. TRASH INCLOSURE - REFER TO DETAIL #9 ON SHEET SP-0.8
34. TRASH INCLOSURE - REFER TO DETAIL #10 ON SHEET SP-1.1
35. TRASH INCLOSURE - REFER TO DETAIL #11 ON SHEET SP-1.1
36. TRASH INCLOSURE - REFER TO DETAIL #12 ON SHEET SP-1.1
37. TRASH INCLOSURE - REFER TO DETAIL #13 ON SHEET SP-1.1
38. TRASH INCLOSURE - REFER TO DETAIL #14 ON SHEET SP-1.1
39. TRASH INCLOSURE - REFER TO DETAIL #15 ON SHEET SP-1.1
40. TRASH INCLOSURE - REFER TO DETAIL #16 ON SHEET SP-1.1
41. TRASH INCLOSURE - REFER TO DETAIL #17 ON SHEET SP-1.1
42. TRASH INCLOSURE - REFER TO DETAIL #18 ON SHEET SP-1.1
43. TRASH INCLOSURE - REFER TO DETAIL #19 ON SHEET SP-1.1
44. TRASH INCLOSURE - REFER TO DETAIL #20 ON SHEET SP-1.1
45. TRASH INCLOSURE - REFER TO DETAIL #21 ON SHEET SP-1.1
46. TRASH INCLOSURE - REFER TO DETAIL #22 ON SHEET SP-1.1
47. TRASH INCLOSURE - REFER TO DETAIL #23 ON SHEET SP-1.1
48. TRASH INCLOSURE - REFER TO DETAIL #24 ON SHEET SP-1.1
49. TRASH INCLOSURE - REFER TO DETAIL #25 ON SHEET SP-1.1
50. TRASH INCLOSURE - REFER TO DETAIL #26 ON SHEET SP-1.1
51. TRASH INCLOSURE - REFER TO DETAIL #27 ON SHEET SP-1.1
52. TRASH INCLOSURE - REFER TO DETAIL #28 ON SHEET SP-1.1
53. TRASH INCLOSURE - REFER TO DETAIL #29 ON SHEET SP-1.1
54. TRASH INCLOSURE - REFER TO DETAIL #30 ON SHEET SP-1.1
55. TRASH INCLOSURE - REFER TO DETAIL #31 ON SHEET SP-1.1
56. TRASH INCLOSURE - REFER TO DETAIL #32 ON SHEET SP-1.1
57. TRASH INCLOSURE - REFER TO DETAIL #33 ON SHEET SP-1.1
58. TRASH INCLOSURE - REFER TO DETAIL #34 ON SHEET SP-1.1
59. TRASH INCLOSURE - REFER TO DETAIL #35 ON SHEET SP-1.1
60. TRASH INCLOSURE - REFER TO DETAIL #36 ON SHEET SP-1.1
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62. TRASH INCLOSURE - REFER TO DETAIL #38 ON SHEET SP-1.1
63. TRASH INCLOSURE - REFER TO DETAIL #39 ON SHEET SP-1.1
64. TRASH INCLOSURE - REFER TO DETAIL #40 ON SHEET SP-1.1
65. TRASH INCLOSURE - REFER TO DETAIL #41 ON SHEET SP-1.1
66. TRASH INCLOSURE - REFER TO DETAIL #42 ON SHEET SP-1.1
67. TRASH INCLOSURE - REFER TO DETAIL #43 ON SHEET SP-1.1
68. TRASH INCLOSURE - REFER TO DETAIL #44 ON SHEET SP-1.1
69. TRASH INCLOSURE - REFER TO DETAIL #45 ON SHEET SP-1.1
70. TRASH INCLOSURE - REFER TO DETAIL #46 ON SHEET SP-1.1
71. TRASH INCLOSURE - REFER TO DETAIL #47 ON SHEET SP-1.1
72. TRASH INCLOSURE - REFER TO DETAIL #48 ON SHEET SP-1.1
73. TRASH INCLOSURE - REFER TO DETAIL #49 ON SHEET SP-1.1
74. TRASH INCLOSURE - REFER TO DETAIL #50 ON SHEET SP-1.1
75. TRASH INCLOSURE - REFER TO DETAIL #51 ON SHEET SP-1.1
76. TRASH INCLOSURE - REFER TO DETAIL #52 ON SHEET SP-1.1
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78. TRASH INCLOSURE - REFER TO DETAIL #54 ON SHEET SP-1.1
79. TRASH INCLOSURE - REFER TO DETAIL #55 ON SHEET SP-1.1
80. TRASH INCLOSURE - REFER TO DETAIL #56 ON SHEET SP-1.1
81. TRASH INCLOSURE - REFER TO DETAIL #57 ON SHEET SP-1.1
82. TRASH INCLOSURE - REFER TO DETAIL #58 ON SHEET SP-1.1
83. TRASH INCLOSURE - REFER TO DETAIL #59 ON SHEET SP-1.1
84. TRASH INCLOSURE - REFER TO DETAIL #60 ON SHEET SP-1.1
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90. TRASH INCLOSURE - REFER TO DETAIL #66 ON SHEET SP-1.1
91. TRASH INCLOSURE - REFER TO DETAIL #67 ON SHEET SP-1.1
92. TRASH INCLOSURE - REFER TO DETAIL #68 ON SHEET SP-1.1
93. TRASH INCLOSURE - REFER TO DETAIL #69 ON SHEET SP-1.1
94. TRASH INCLOSURE - REFER TO DETAIL #70 ON SHEET SP-1.1
95. TRASH INCLOSURE - REFER TO DETAIL #71 ON SHEET SP-1.1
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97. TRASH INCLOSURE - REFER TO DETAIL #73 ON SHEET SP-1.1
98. TRASH INCLOSURE - REFER TO DETAIL #74 ON SHEET SP-1.1
99. TRASH INCLOSURE - REFER TO DETAIL #75 ON SHEET SP-1.1
100. TRASH INCLOSURE - REFER TO DETAIL #76 ON SHEET SP-1.1
101. TRASH INCLOSURE - REFER TO DETAIL #77 ON SHEET SP-1.1
102. TRASH INCLOSURE - REFER TO DETAIL #78 ON SHEET SP-1.1
103. TRASH INCLOSURE - REFER TO DETAIL #79 ON SHEET SP-1.1
104. TRASH INCLOSURE - REFER TO DETAIL #80 ON SHEET SP-1.1
105. TRASH INCLOSURE - REFER TO DETAIL #81 ON SHEET SP-1.1
106. TRASH INCLOSURE - REFER TO DETAIL #82 ON SHEET SP-1.1
107. TRASH INCLOSURE - REFER TO DETAIL #83 ON SHEET SP-1.1
108. TRASH INCLOSURE - REFER TO DETAIL #84 ON SHEET SP-1.1
109. TRASH INCLOSURE - REFER TO DETAIL #85 ON SHEET SP-1.1
110. TRASH INCLOSURE - REFER TO DETAIL #86 ON SHEET SP-1.1
111. TRASH INCLOSURE - REFER TO DETAIL #87 ON SHEET SP-1.1
112. TRASH INCLOSURE - REFER TO DETAIL #88 ON SHEET SP-1.1
113. TRASH INCLOSURE - REFER TO DETAIL #89 ON SHEET SP-1.1
114. TRASH INCLOSURE - REFER TO DETAIL #90 ON SHEET SP-1.1
115. TRASH INCLOSURE - REFER TO DETAIL #91 ON SHEET SP-1.1
116. TRASH INCLOSURE - REFER TO DETAIL #92 ON SHEET SP-1.1
117. TRASH INCLOSURE - REFER TO DETAIL #93 ON SHEET SP-1.1
118. TRASH INCLOSURE - REFER TO DETAIL #94 ON SHEET SP-1.1
119. TRASH INCLOSURE - REFER TO DETAIL #95 ON SHEET SP-1.1
120. TRASH INCLOSURE - REFER TO DETAIL #96 ON SHEET SP-1.1
121. TRASH INCLOSURE - REFER TO DETAIL #97 ON SHEET SP-1.1
122. TRASH INCLOSURE - REFER TO DETAIL #98 ON SHEET SP-1.1
123. TRASH INCLOSURE - REFER TO DETAIL #99 ON SHEET SP-1.1
124. TRASH INCLOSURE - REFER TO DETAIL #100 ON SHEET SP-1.1

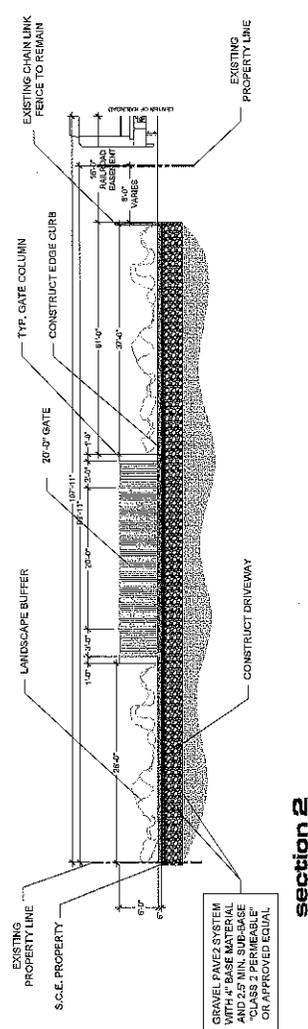


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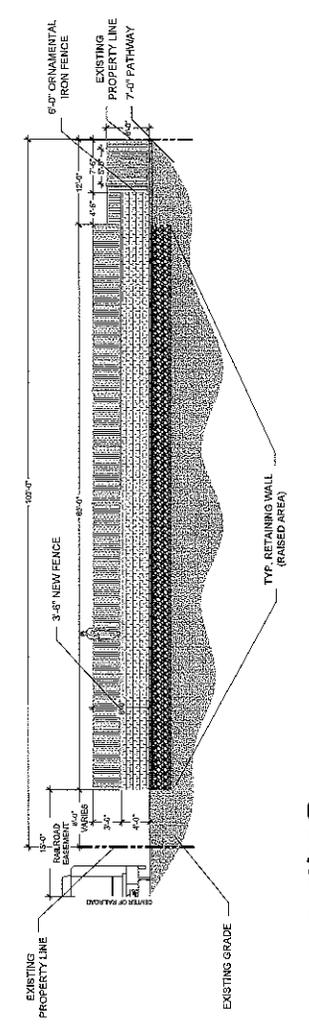
| | ENGINEER RICK ENGINEERING COMPANY 27777 Harbor Avenue, Suite 200 Irvine, CA 92617 Phone: 949.261.1111 Fax: 949.261.1112 | ARCHITECT BIR ARCHITECTS 285 East 17th Street, Suite 300 San Diego, CA 92101 Phone: 619.594.1111 | REVISIONS <table border="1"> <tr><th>NO.</th><th>DATE</th><th>REVISIONS</th></tr> <tr><td> </td><td> </td><td> </td></tr> </table> | NO. | DATE | REVISIONS | | | | | | | | | | | | | | | | | | | | | | | | | | | | SHEET NUMBER SP-0.5 |
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| CONSULTANT CALIFORNIA PROFESSIONAL ENGINEERS 27777 Harbor Avenue, Suite 200 Irvine, CA 92617 Phone: 949.261.1111 Fax: 949.261.1112 | LANDSCAPE ARCHITECT CALIFORNIA LANDSCAPE ARCHITECTS 27777 Harbor Avenue, Suite 200 Irvine, CA 92617 Phone: 949.261.1111 Fax: 949.261.1112 | SKATE PARK OPS SIEVE ENGINEERING, INC. 27777 Harbor Avenue, Suite 200 Irvine, CA 92617 Phone: 949.261.1111 Fax: 949.261.1112 | SKATE PARK OPS SIEVE ENGINEERING, INC. 27777 Harbor Avenue, Suite 200 Irvine, CA 92617 Phone: 949.261.1111 Fax: 949.261.1112 | VANS HUNTINGTON BEACH SKATE PARK + RETAIL Located at Project Address: Between Center Ave. and McFadden Ave. east of Cotbard St. | Site Plan 2 of 2 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |



section 1



section 2



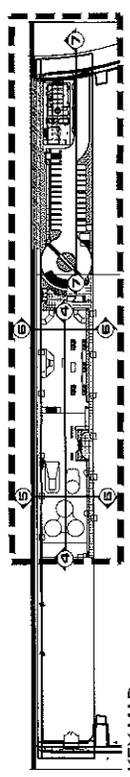
section 3

GRAVEL PAVEZ SYSTEM WITH 4\"/>

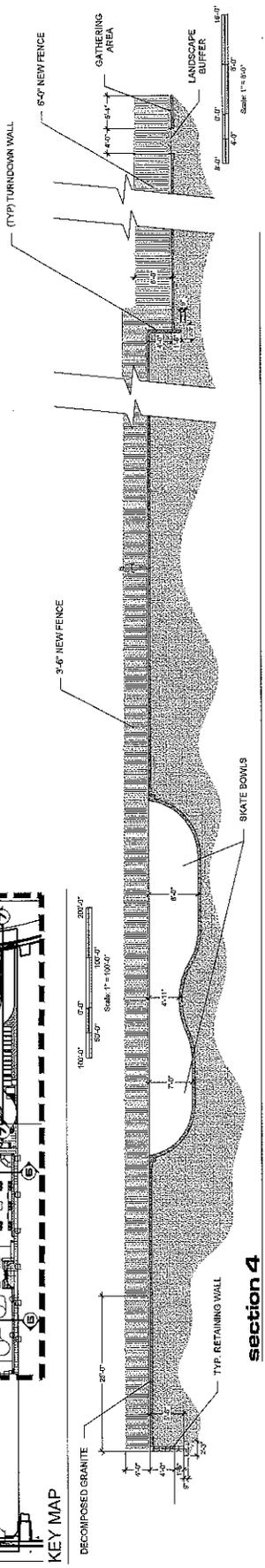
GRAVEL PAVEZ SYSTEM WITH 4\"/>

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| | ENGINEER RICK ENGSTROM ARCHITECT 200 S. Main Street, Suite 200 San Francisco, CA 94108 Phone: (415) 398-1222 | DATE 11/11/2021 | BY [Signature] | REVISIONS | NO. | SHEET NUMBER SP-0.6 |
| | PROJECT ADDRESS: Located on the Edinger Corridor between Center Ave. and McFadden Ave. east of Gothard St. | VANS HUNTINGTON BEACH SKATE PARK + RETAIL Section Profiles | | | | |

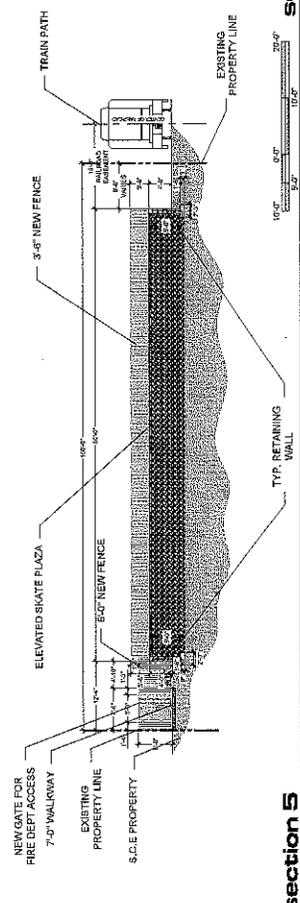
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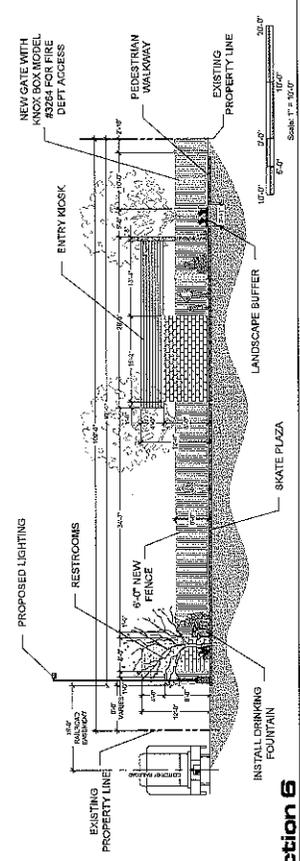
KEY MAP



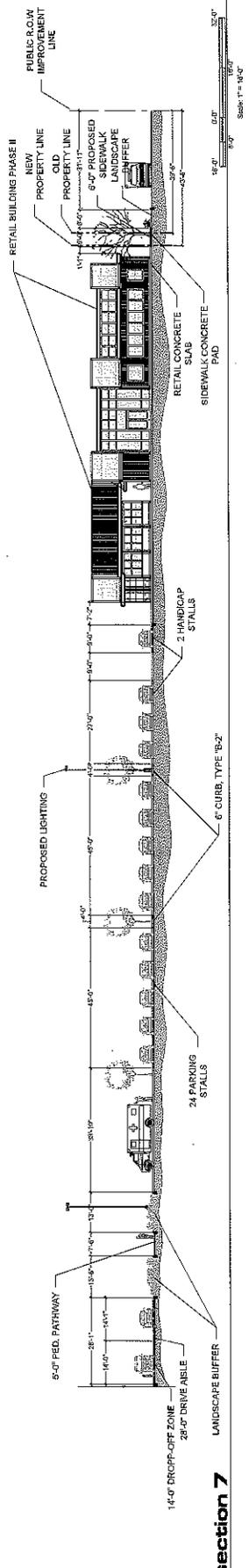
section 4



section 5



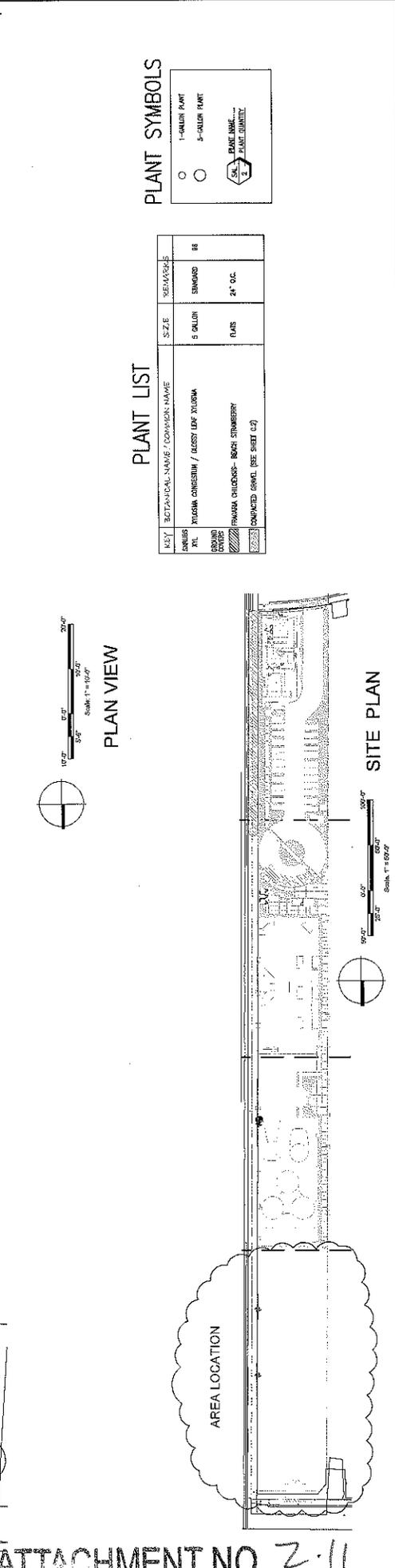
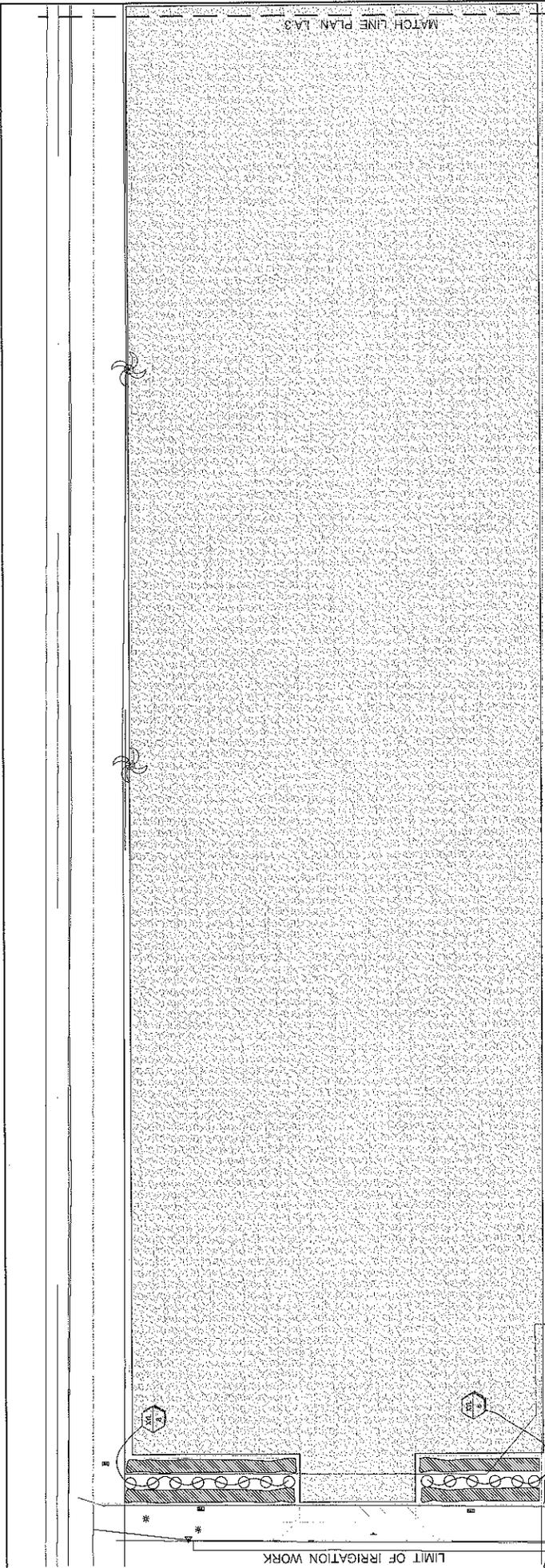
section 6



section 7

ATTACHMENT NO. 2.6

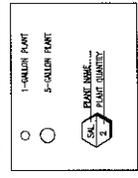
| PRIME CONSULTANT | CALIFORNIA STATE ENGINEER 2777 West Alhambra Way, Suite 100 Alhambra, CA 91803 Phone: 627-228-2210 Fax: 627-228-2211 www.ces-engineers.com | LANDSCAPE DESIGNER CALIFORNIA LANDSCAPE ARCHITECT 2777 West Alhambra Way, Suite 100 Alhambra, CA 91803 Phone: 627-228-2210 Fax: 627-228-2211 www.ces-engineers.com | COUL ENGINEER TRICK CIVIL ENGINEERING COMPANY 1770 West Alhambra Way, Suite 100 Alhambra, CA 91803 Phone: 627-228-2210 Fax: 627-228-2211 www.ces-engineers.com | ELECTRICAL ENGINEER BIRIR CONSULTING 100 S. Bascom Avenue, Suite 200 San Francisco, CA 94108 Phone: 415-764-6032 | ARCHITECT BIRIR ARCHITECTS 100 S. Bascom Avenue, Suite 200 San Francisco, CA 94108 Phone: 415-764-6032 | REVISIONS <table border="1"> <tr><th>NO.</th><th>DATE</th><th>BY</th></tr> <tr><td> </td><td> </td><td> </td></tr> </table> | NO. | DATE | BY | | | | | | | | | | | | | | | | | | | | | | | | | | | | Site Plan Submittal | SHEET NUMBER SP-0.7 |
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| VANS HUNTINGTON BEACH SKATE PARK + RETAIL PROJECT ADDRESS: Located on the Edinger Corridor Between Center Ave. and McAdden Ave. east of Goldard St. | | | | | | | Section Profiles | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |



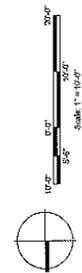
PLANT LIST

| KEY | SYMBOL | RECTANGULAR NAME / COMMON NAME | SIZE | REMARKS |
|-----|--------|--------------------------------------|----------|------------|
| 1 | ○ | YUCCA CONSISTENS / GLOSSY LEAF YUCCA | 5 GALLON | SPACING 88 |
| 2 | ○ | PHOENIX QUERCUS - BUSH STRAUBER | PLANT | 24" O.C. |
| 3 | ○ | COMPACT DRIVEL (SEE SHEET 4) | | |

PLANT SYMBOLS



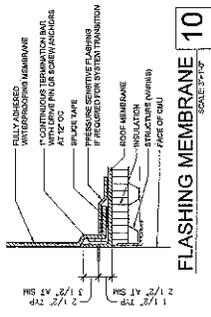
PLAN VIEW



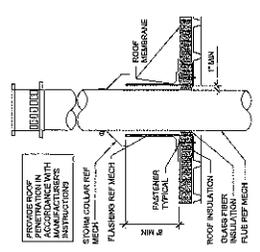
SITE PLAN



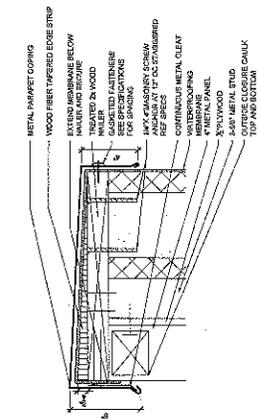
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| | SECTIONS | | | | | | | |
| SV | DATE | NO. | VANS HUNTINGTON BEACH SKATE PARK + RETAIL | | | | | SITE PLAN SUBMITTAL |
| | | | PROJECT ADDRESS: Located on the Edinger Corridor Between Center Ave. and McRadden Ave. east of Goldard St. | | | | | LANDSCAPE PLANTING PLAN PART 4 |
| | | | SIT 2727 VAN ALLEN STREET, VANOC, CA 91766 916-251-7000 www.sitinc.com | | | | | LA.4 |
| | | | CALIFORNIA skateparks 273 North Benson Avenue Upland, California 91786 Phone: 1.800.CA.SKATE Fax: 909.949.1601 | | | | | SHEET NUMBER |



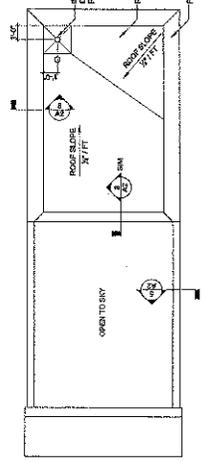
FLASHING MEMBRANE 10
SCALE: 1/2" = 1'-0"



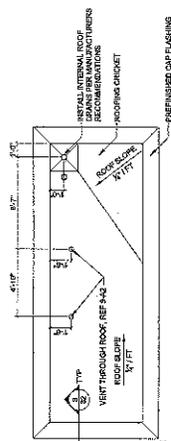
ROOF VENT 9
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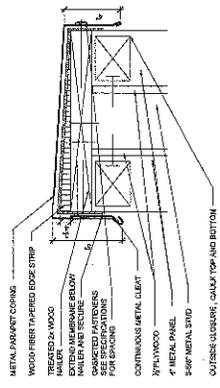
FLASHING 8
SCALE: 1/2" = 1'-0"



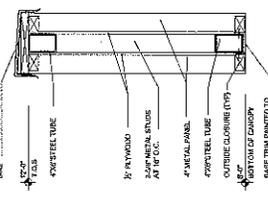
KIOSK ROOF PLAN 7
SCALE: 1/2" = 1'-0"



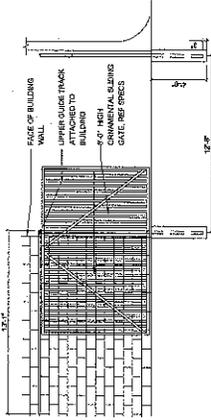
RESTROOM ROOF PLAN 6
SCALE: 1/2" = 1'-0"



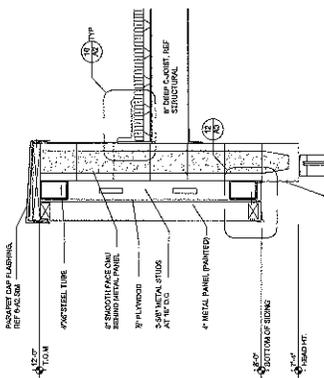
FLASHING 5
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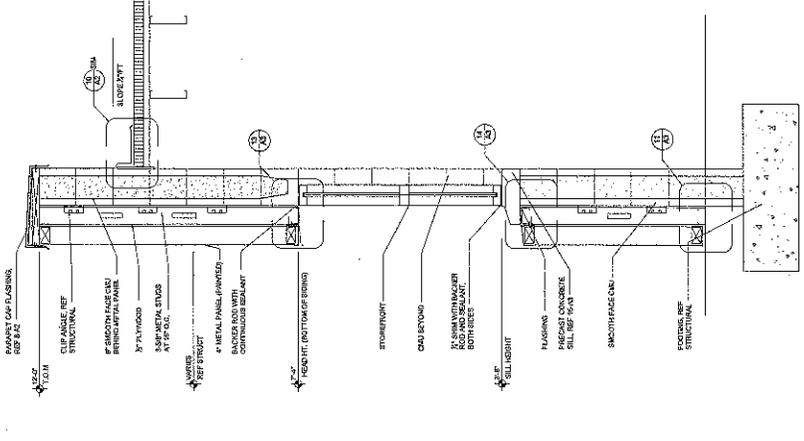
SIDING DETAIL 4
SCALE: 1/2" = 1'-0"



KIOSK GATE 3
SCALE: 1/2" = 1'-0"



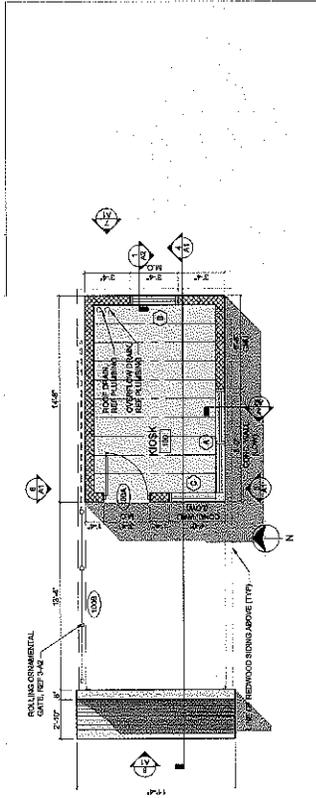
WALL SECTION 2
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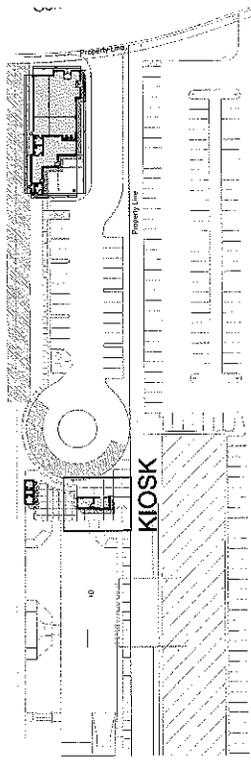
WALL SECTION 1
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ATTACHMENT NO. 2.13

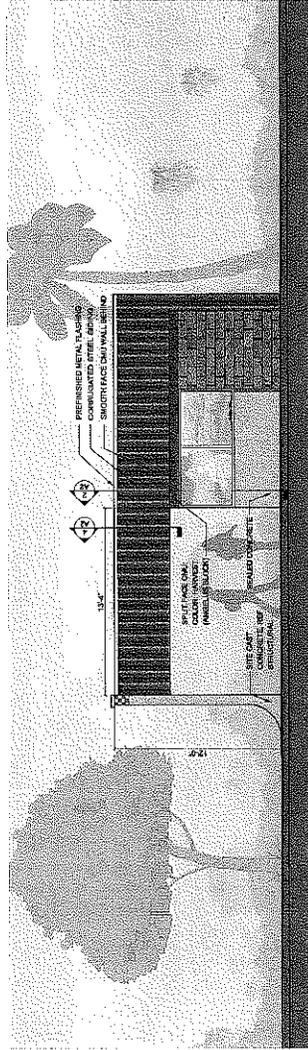
| | | | | | | | | | | | |
|--|--|--|--|---|--|---|--|---|--|-------------------------------------|--|
| <p>CALIFORNIA Skateparks 273 North Benson Avenue Upland, California 91786 Phone: 1.800.CA.SKATE Fax: 909.949.1601</p> | | <p>SIL DESIGN GROUP, INC. 2777 Lake Avenue S. Suite 5 91734-2510 www.sil-design.com</p> | | <p>B R R ARCHITECTS 1000 S. GARDEN ST. SUITE 100 POMONA, CA 91768 PH: 909.861.1111</p> | | <p>VANS HUNTINGTON BEACH SKATE PARK + RETAIL PROJECT ADDRESS: Located on the Edinger Corridor Between Center Ave. and Macraeden Ave. east of Goldard St.</p> | | <p>SITE PLAN SUBMITTAL WALL SECTIONS AND ROOF PLANS</p> | | <p>SHEET NUMBER A2.0</p> | |
| <p>DOCUMENT DATE: 09/26/12</p> | | <p>BY: [Signature]</p> | | <p>REVISIONS:</p> | | <p>NO.</p> | | <p>DATE</p> | | <p>DESCRIPTION</p> | |



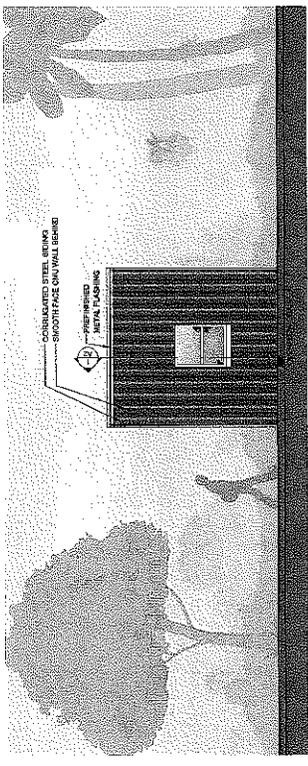
PLAN



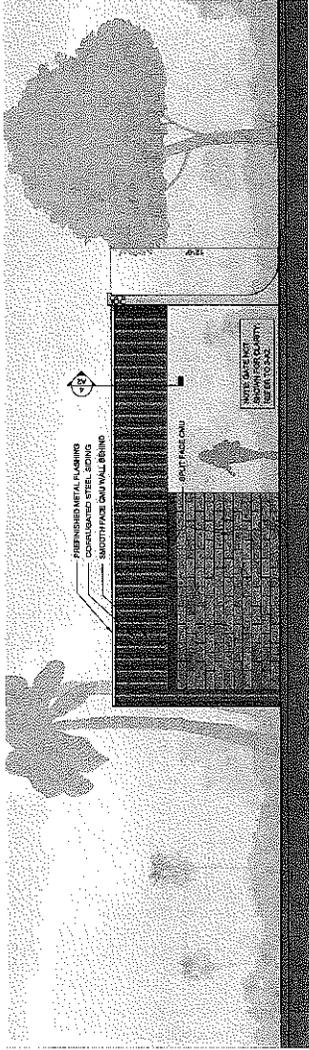
KEY PLAN - not to scale



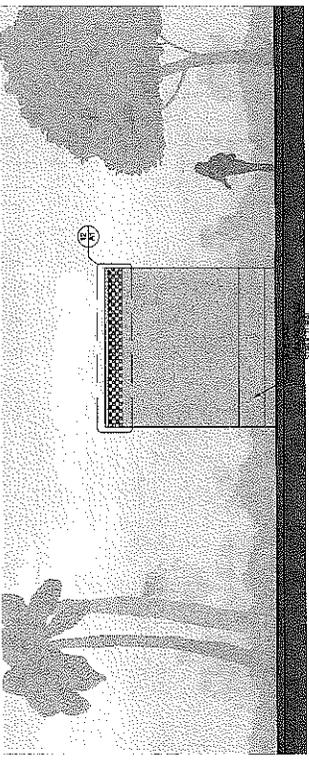
SOUTH ELEVATION



EAST ELEVATION



NORTH ELEVATION



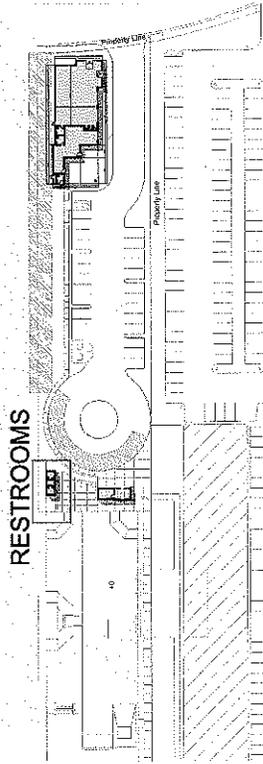
WEST ELEVATION



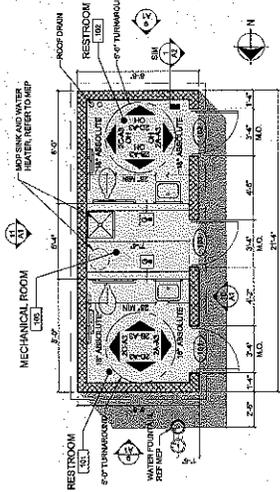
ATTACHMENT NO. 2.14

| | | | | |
|---|--|---|--------------------------|----------------------------|
|  BIRK ARCHITECTURE 282 South Street, Suite 200 San Francisco, CA 94107 Phone: 415.774.1111 Fax: 415.774.1124 | ENGINEER 40 S Street, Suite 200 San Diego, CA 92101 Phone: 619.441.1111 Fax: 619.441.1124 | ARCHITECT 40 S Street, Suite 200 San Diego, CA 92101 Phone: 619.441.1111 Fax: 619.441.1124 | REVISIONS NO. DATE BY | SHEET NUMBER A 4 |
| | | | | |

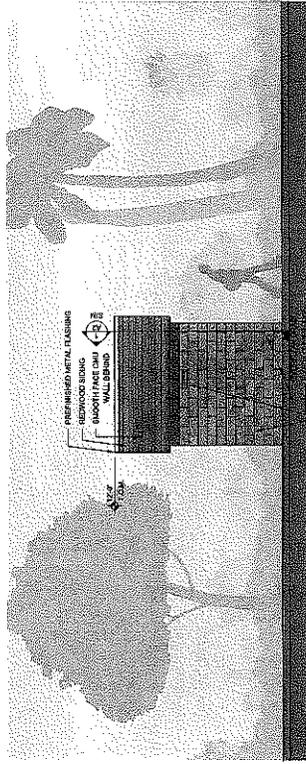
RESTROOMS



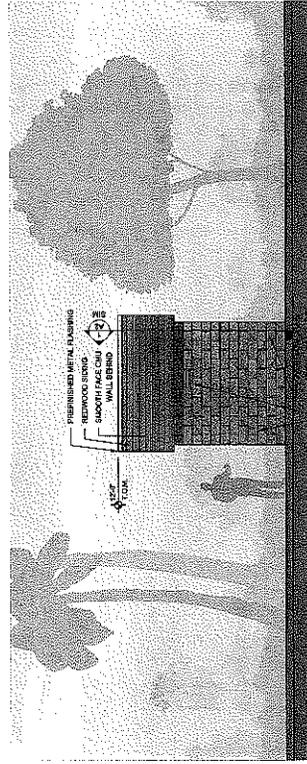
KEY PLAN - not to scale



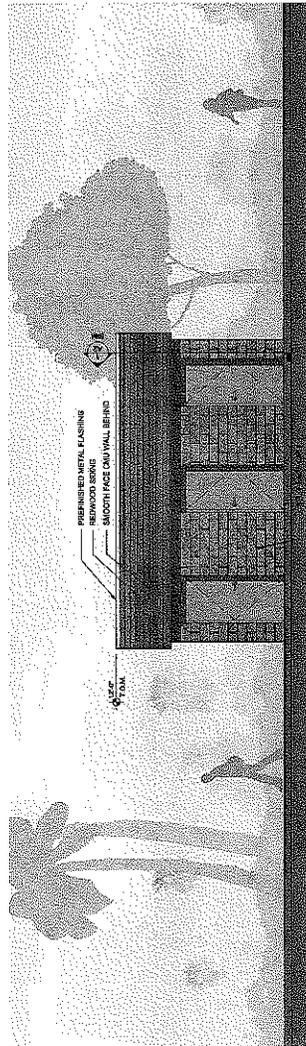
PLAN



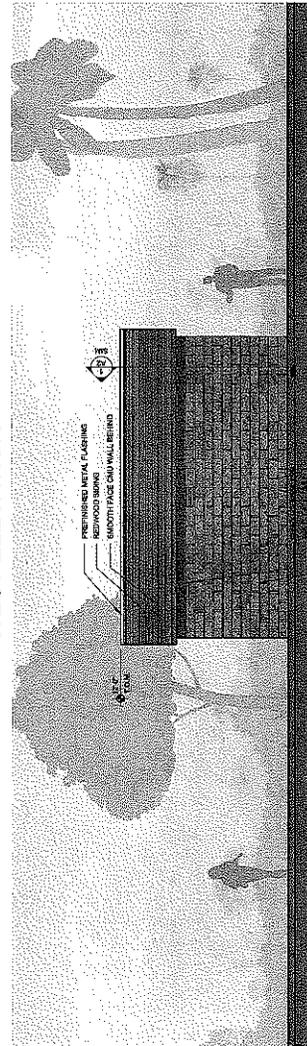
NORTH ELEVATION



SOUTH ELEVATION



WEST ELEVATION



EAST ELEVATION



| | | | | | |
|---|---|--|--|--|--|
| | DATE _____ BY _____ REVISIONS: | NO. _____ REVISIONS: | SHEET NUMBER A 5 | | |
| | VANS HUNTINGTON BEACH SKATE PARK + RETAIL PROJECT ADDRESS: Located on the Edinger Corridor, Between Center Ave. and Ingraham Ave, East of Gothard St. | | | Site Plan Submittal Restrooms Renders | |
| PRIME CONSULTANT | SKATE PARK CONSULTANT | ROADSIDE DESIGNER | CIVIL ENGINEER | ELECTRICAL ENGINEER | ARCHITECT |
| SUE CALIFORNIA ENGINEERING COMPANY 2777 Lundy Avenue, Suite 100 San Diego, CA 92108 Phone: 619-444-2271 Fax: 619-444-2272 | CALIFORNIA ENGINEERING COMPANY 2777 Lundy Avenue, Suite 100 San Diego, CA 92108 Phone: 619-444-2271 Fax: 619-444-2272 | CA CALIFORNIA ENGINEERING COMPANY 2777 Lundy Avenue, Suite 100 San Diego, CA 92108 Phone: 619-444-2271 Fax: 619-444-2272 | ENGINEER Rick Engineering Company 200 San Marcos Avenue, Suite 200 San Marcos, CA 92069 Phone: 760-439-0017 Fax: 760-439-0018 | ARCHITECT BIRR Architects 200 San Marcos Avenue, Suite 200 San Marcos, CA 92069 Phone: 760-439-0017 Fax: 760-439-0018 | PROJECT NUMBER VANS HUNTINGTON BEACH SKATE PARK + RETAIL |



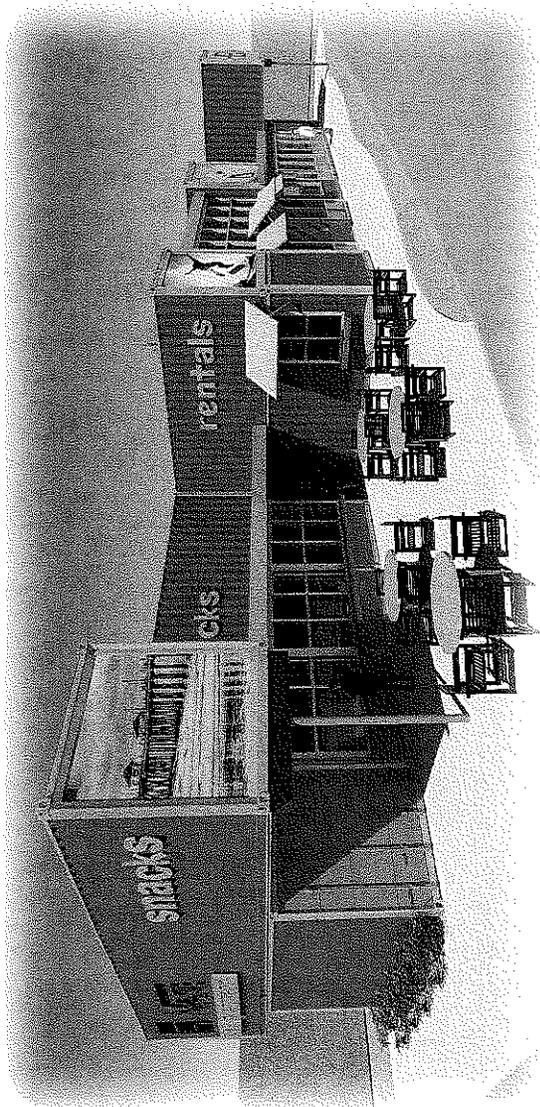
STREET PERSPECTIVE



FRONT (RETAIL) PERSPECTIVE

ATTACHMENT NO. 2.16

| | | | | | | | | | |
|--|--|---|---|-----------------------|------------|-----|---|---------------|-----------------------------|
|  CALIFORNIA skateparks 270 North Borsari Avenue, Upland California, 91786 Phone: 1.800.CA-SKATE Fax: 951.949.1801 |  DESIGN GROUP, INC. 270 North Borsari Avenue, Upland, CA 91786 P: 951.949.1801 F: 951.949.1801 www.sitelogroup.com | TODD B. SPIEGEL ARCHITECTS 8100 Wilshire Center Road Westlake Village, CA 91361 J 818.375.6175 D 818.375.0339 |  | BY: _____ DATE: _____ | REVISIONS: | No. | VANS HUNTINGTON BEACH SKATE PARK + RETAIL PROJECT ADDRESS: Located on the Eclinger Corridor Between Center Ave. and McFadden Ave. east of Gothard St. | CUP SUBMITTAL | SHEET NUMBER R1.1 |
| | | | | PERSPECTIVES | | | | | |



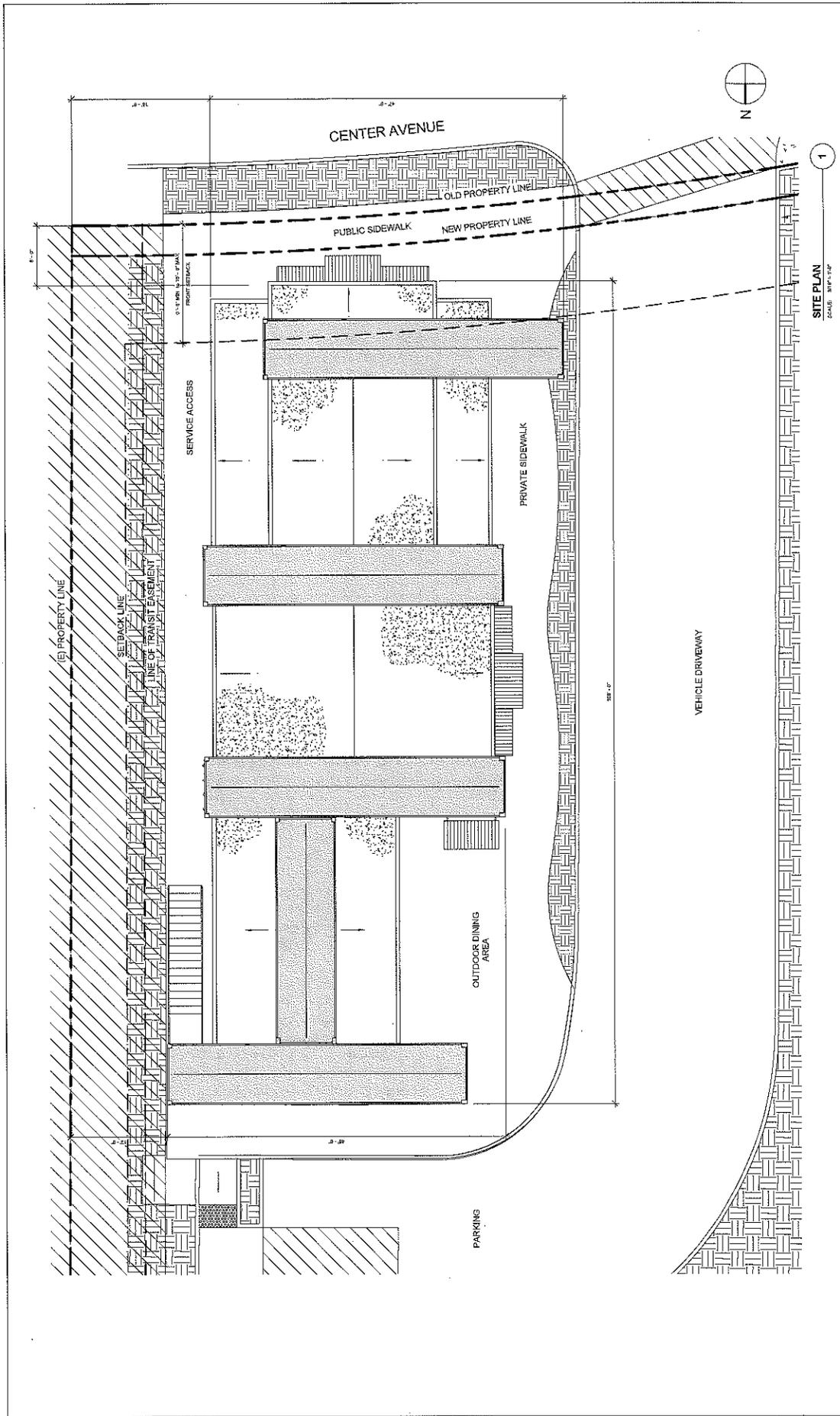
FRONT (FOOD SERVICE) PERSPECTIVE



REAR PERSPECTIVE

ATTACHMENT NO. 2.17

| | | | | | | | | | | | | |
|---|------|---|-----|---|--|---|--|--|--|---|--|-----------------------------|
|  CALIFORNIA skateparks 273 North Berson Avenue Upland California 91786 Phone: 1.800.CA.SKATE Fax: 909.948.1501 | |  S&TE DESIGN GROUP, INC. 277 Lake Street W. Ste 0 Burlingame, CA 94010 Phone: 415.333.2270 www.s&tedesigngroup.com | | TODD B. SPIEGEL ARCHITECTS 31223 Village Center Road Washke, CA 91081 818.378.8716 818.879.0289 | |  | | VANS HUNTINGTON BEACH SKATE PARK + RETAIL PROJECT ADDRESS: Located on the Edinger Corridor Between Center Ave. and McFadden Ave. east of Goffard St. | | CUP SUBMITTAL PERSPECTIVES | | SHEET NUMBER R1.2 |
| BY | DATE | REVISIONS | No. | | | | | | | | | |
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SITE PLAN
SCALE: 1/8" = 1'-0"

1

SHEET NUMBER: R2.1

CUP SUBMITTAL

SITE PLAN

VANS HUNTINGTON BEACH SKATE PARK + RETAIL

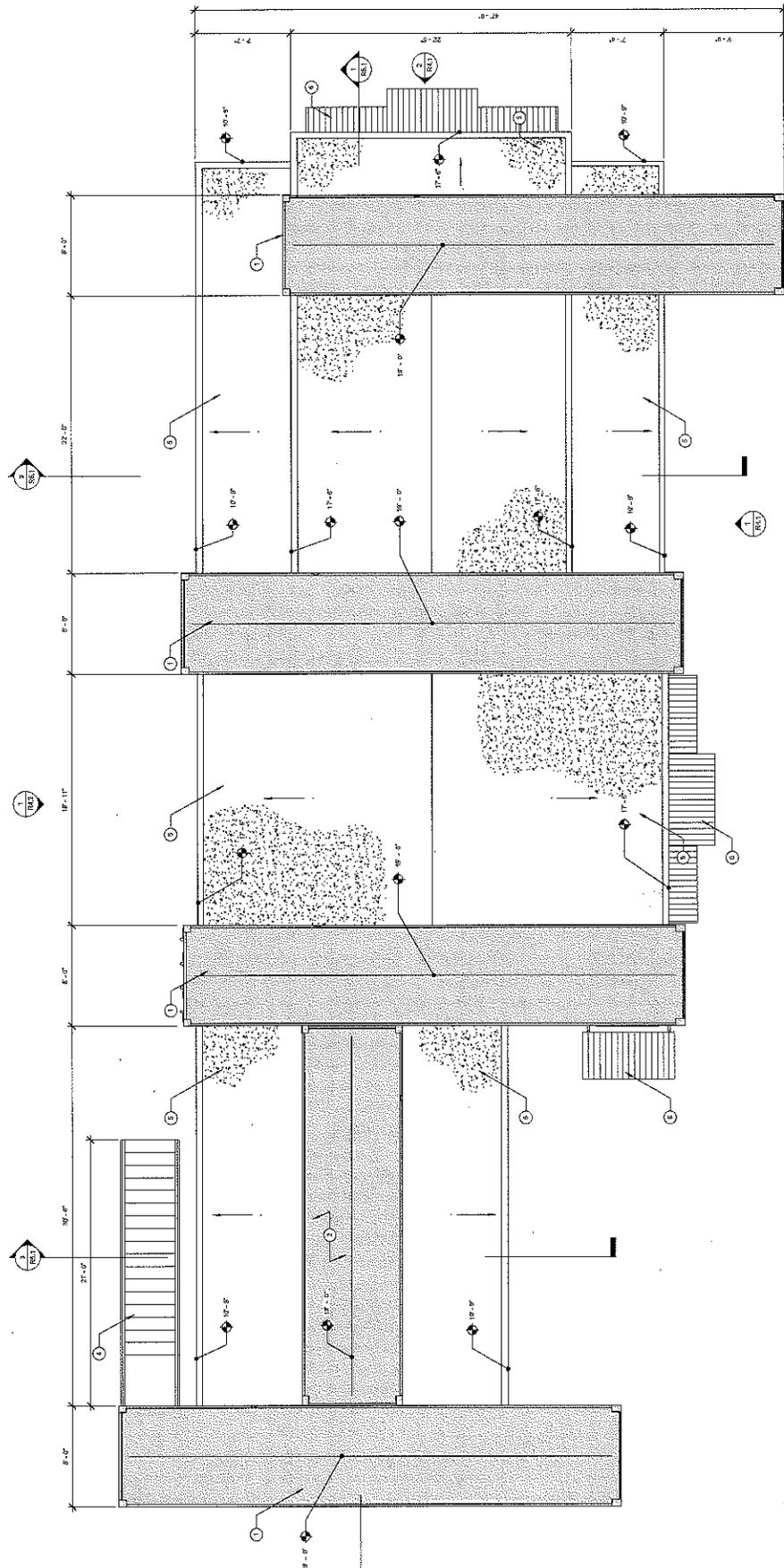
PROJECT ADDRESS:
Located on the Edinger Corridor Between
Center Ave. and McFadden Ave. east of Gobard St.

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TODD B. SPIEGEL ARCHITECTS
31822 Village Center Road
Westlake Village, CA 91391
CONTACT: T. SPIEGEL
P: 818.273.0359
D: 818.273.0359

CALIFORNIA SITE skatemarks
DESIGN GROUP, INC.
273 North Benson Avenue
Upland California 91786
Phone: 1.800.CA.SKATE | Fax: 909.948.1601

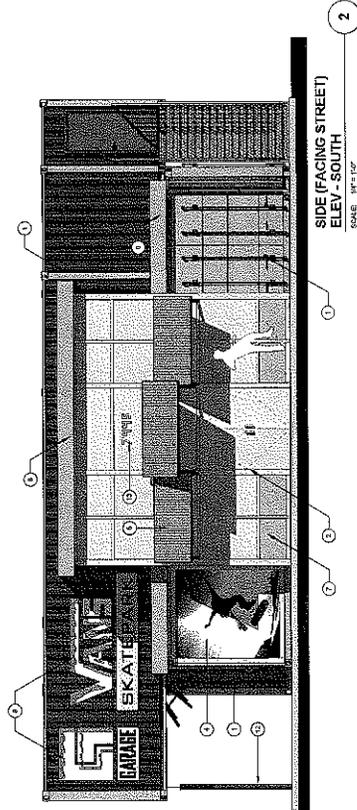
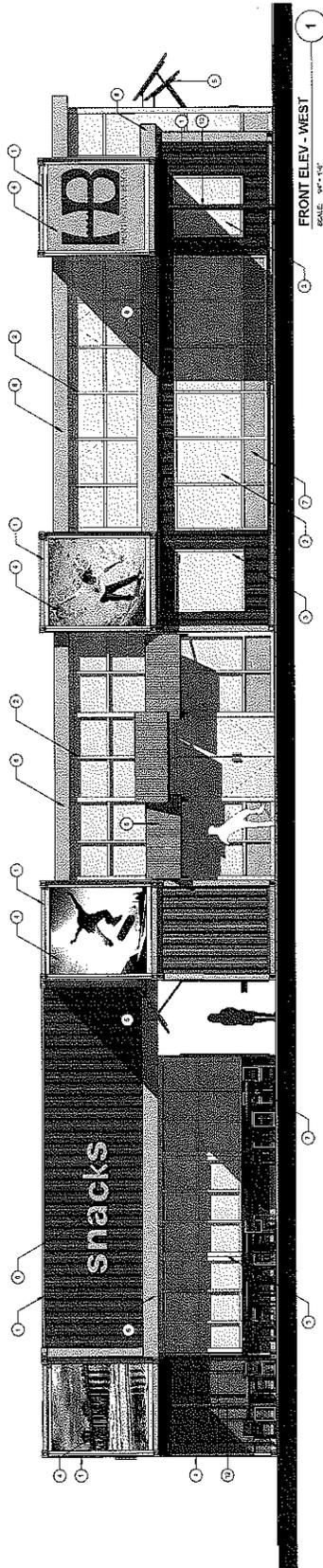
Professional Seal: Todd B. Spiegel, Architect, No. 12345, State of California



- REVISED**
- 1 RECYCLED 60% CORRUGATED STEEL SHIPPING CONTAINER, PAINTED
 - 2 RECYCLED 30% CORRUGATED STEEL SHIPPING CONTAINER, PAINTED
 - 3 RECYCLED 20% CORRUGATED STEEL SHIPPING CONTAINER, PAINTED
 - 4 STEEL STAIRS/GENERAL PAINTED
 - 5 BUILT-UP FLOORING WITH GRAVEL/COATING
 - 6 CORRUGATED METAL AWNING, PAINTED

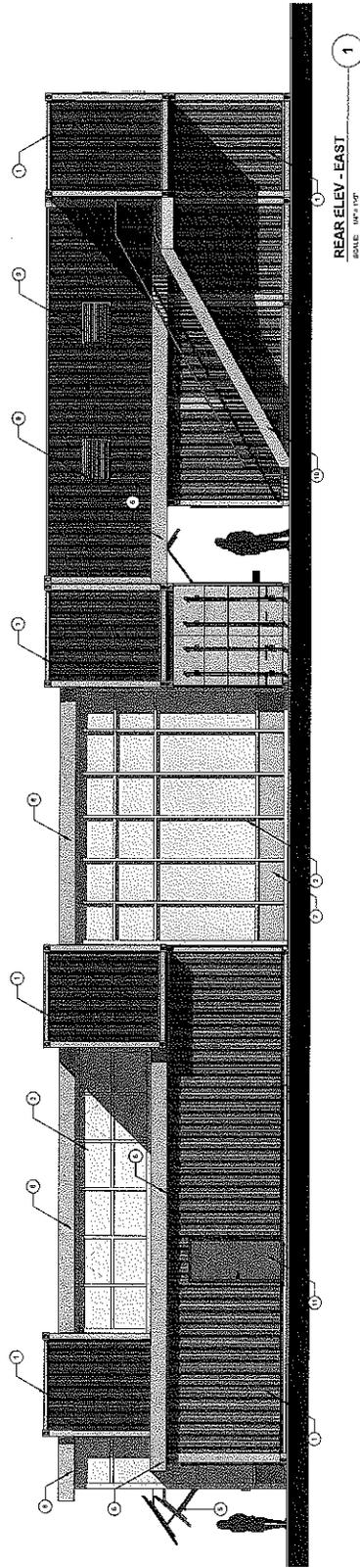


| CALIFORNIA skateparks | DESIGN GROUP, INC. | TODD B. SPIEGEL ARCHITECTS | VANS HUNTINGTON BEACH SKATE PARK + RETAIL | CUP SUBMITTAL | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|--|--|---|----------------------|----|-----------|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|---|--|
| 273 North Benson Avenue Upland, California 91766 Phone: 1-800-CA-SKATE Fax: 909-949-1601 | 34923 Wilton Center Road Westlake Village, CA 91361 P 818-879-9176 D 818-373-0539 | 34923 Wilton Center Road Westlake Village, CA 91361 P 818-879-9176 D 818-373-0539 | Located on the Edinger Corridor Between Center Ave. and McFadden Ave. east of Golfhard St. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <p>REVISIONS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>BY</th> <th>REVISIONS</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table> | | | NO. | DATE | BY | REVISIONS | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | <p>SHEET NUMBER R3.3</p> <p style="text-align: center;">ROOF PLAN</p> | |
| NO. | DATE | BY | REVISIONS | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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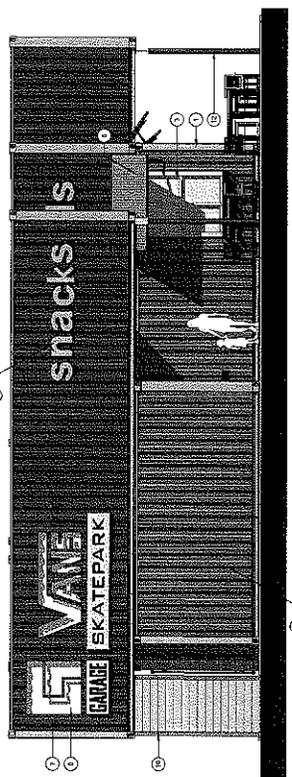


- NOTES**
- 1 RECYCLED COLORMATED STEEL SHIPPING CONTAINER, PAINTED
 - 2 ALUMINUM STOREFRONT, PAINTED, CLEAR GLASS
 - 3 ALUMINUM WINDOW, PAINTED, CLEAR GLASS
 - 4 ALUMINUM WINDOW, PAINTED, TINTED AWT GLASS (OR SKY GLASS)
 - 5 CORRUGATED METAL PANEL, PAINTED
 - 6 HEAVY GAUGE METAL PANEL, PAINTED
 - 7 HEAVY GAUGE METAL PANEL, PAINTED
 - 8 BUILDING EXTERIOR WOOD PANELING (REVIEW & APPROVAL, METAL CLADDING TO MEET FIRE RESISTANCE)
 - 9 METAL PANEL, PAINTED
 - 10 STEEL STRUCTURE, PAINTED
 - 11 PRELUMINATED SERVICES (WALL, PAINTED)
 - 12 STEEL COLUMN, PAINTED
 - 13 SEE ADDRESS NUMBER PER CITY REGISTRATION

| | | | | | | | |
|---|--|---|---|---|--|-----------------------------|------------------------------|
|  | CALIFORNIA skateparks 273 North Benson Avenue, Upland California 91786 Phone: 1.800.CA.SKATE Fax: 909.545.1601 |  | TODD B. SPIEGEL ARCHITECTS 31522 Village Center Road Westlake Village, CA 91361 D 818.873.0379 |  | VANS HUNTINGTON BEACH SKATE PARK + RETAIL PROJECT ADDRESS: Behrens Center Ave. and 11th Street, east of Gothard St. | CUP SUBMITTAL ELEVATIONS | SHEET NUMBER R4.1C |
|---|--|---|---|---|--|-----------------------------|------------------------------|



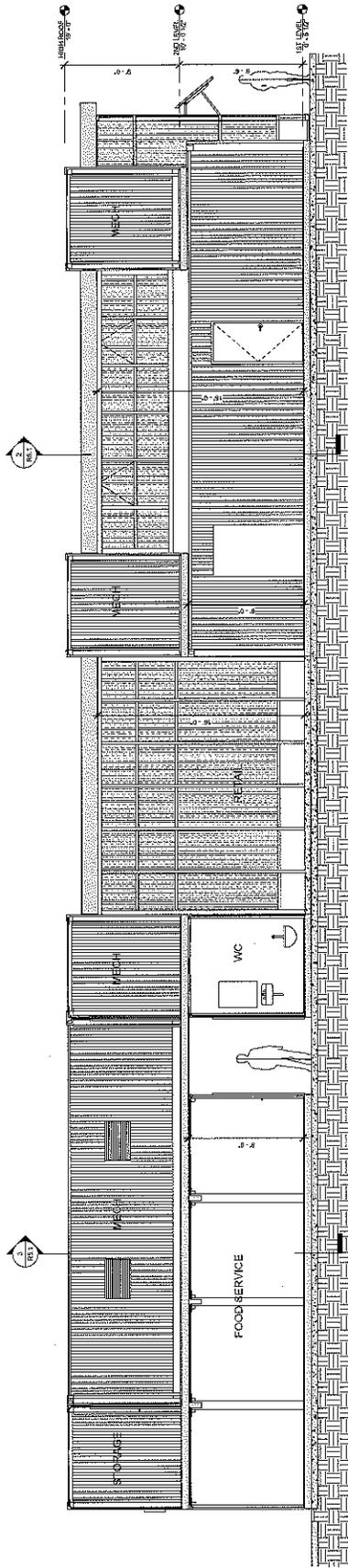
REAR ELEV - EAST
SCALE: 1/8" = 1'-0"



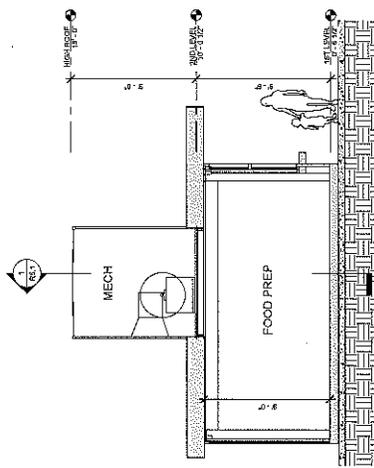
SIDE (FACING SKATEPARK) ELEV - NORTH
SCALE: 1/8" = 1'-0"

- LEGEND**
- 1 RECYCLED COLOURED STEEL SHIPPING CONTAINER, PAINTED
 - 2 ALUMINUM EXTRUSION, PAINTED, CLEAR GLASS
 - 3 ALUMINUM WINDOW, PAINTED, CLEAR GLASS
 - 4 ALUMINUM WINDOW, PAINTED, TYPICAL BULK GLASS
 - 5 COLOURED METAL FINISH, PAINTED
 - 6 HEAVY GRADE METAL FINISH, PAINTED
 - 7 HEAVY GRADE METAL PANEL, PAINTED
 - 8 BUILDING SIGNAGE PANEL, SEPARATE SIGNAGE APPROVAL, METAL & COURTYE CUT, NEAR ILLUMINATION
 - 9 MECHANICAL LOUVER, PAINTED
 - 10 STEEL STAIRWELL, PAINTED
 - 11 HOLLOW METAL SPRING DOOR, PAINTED
 - 12 STEEL COLUMN, PAINTED
 - 13 8" TALL ADDRESS SIGNAGE PANEL APPROVAL SIGNAGE

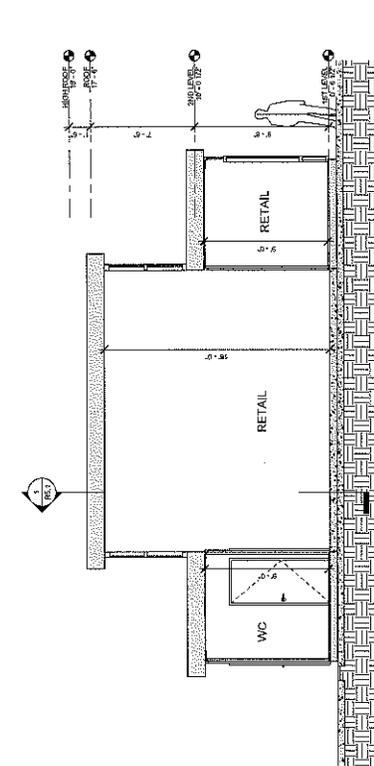
| | | | | | |
|---|---|--|---|--|-------------------------------|
| CALIFORNIA skateparks 273 North Bunker Avenue, Ukiah, California, 97766 Phone: 1.800.CA.SKATE Fax: 309.346.1601 | site DESIGN GROUP, INC. 5700 S. 20th Street Phoenix, AZ 85032 602.997.8888 | TODD B. SPIEGEL ARCHITECTS 3152 Village Center Road Westland, CA 91385 618.872.0388 | REVISIONS No. _____ BY: _____ DATE: _____ | PROJECT ADDRESS: Located on the Elevation Corridor Between Center Ave. and McFadden Ave. east of Goffard St. | SHEET NUMBER: R4.2C |
| | | | CUP SUBMITTAL ELEVATIONS | | |



SECTION 1
SCALE: 1/8" = 1'-0"



SECTION 2
SCALE: 1/8" = 1'-0"



SECTION 3
SCALE: 1/8" = 1'-0"

ATTACHMENT NO. 2.24

| | | | | | | | | |
|--|---|--|---|--------------------------|----------------------------|--|----------------------|-------------|
|  CALIFORNIA skateparks 275 North Bonessa Avenue, Upland, California 91786 Phone: 1.800.CA.SKATE Fax: 909.949.1601 |  SFE SPECIALTY FENCIBLES, INC. 2771 East Palmdale Ave., Suite B Palmdale, CA 91361 Tel: 818.734.7232 Fax: 818.734.7233 www.sfeinc.com | TODD B. SPIEGEL ARCHITECTS 51625 Via de la Cumbre, #104 Woodside, CA 94097 Tel: 818.979.6175 Fax: 818.279.8239 |  | BY: _____ DATE: _____ | REVISIONS: _____ NO. _____ | VANS HUNTINGTON BEACH SKATE PARK + RETAIL PROJECT ADDRESS: Located on the Estingler Corridor Between Center Ave. and McFadden Ave. east of Gothard St. | CUP SUBMITTAL | R5.1 |
| | | | | BUILDING SECTIONS | | SHEET NUMBER | | |



PROJECT NARRATIVE

RECEIVED

SEP 27 2012

Dept. of Planning
& Building

CONDITIONAL USE PERMIT

VARIANCE



CENTER AVE SKATE PARK PROJECT NARRATIVE

Entitlement Request

The applicant requests a Site Plan Review, Conditional Use Permit (C.U.P.), & Variances for the development of a skate park, retail building & concession area (food service) on a 2.718 acre parcel identified as APN 142-073-03 located on the Beach and Edinger Corridor between Center Avenue and McFadden Avenue, east of Gothard Street.

Project Description:

VF Outdoor, Inc. (VANS) is leasing the currently vacant property from the City of Huntington Beach to design, develop, maintain and operate a public skate park. The project shall include a supervised facility containing 14,500 square foot skate park plaza area, 11,800 square feet of skate bowl area, 13,700 square feet of walkways and concrete entrances, 10,300 square feet of landscape area, and a 3,500 square foot operations building. The retail building will include but is not limited to pre-packaged concession stand (food service), skate rental space, employee office space, employee restrooms, and storage. Additional to the skate park & retail building there would be a secured 185 square foot 2 stall unisex restroom structure, a 150 square foot skate park entrance kiosk, the main parking lot near the primary site access perpendicular to Center Ave. frontage, containing 24 parking stalls and 2 Handicap stalls, and a gated staging area off McFadden Ave. to be used only for special event employees & event vendors. The project will also include ADA sidewalks / walkways, gathering areas, trash/recycling facilities, drinking fountains, bike racks, trash enclosure, drop off zone, parking area and skate park lighting, and all of which would be accessible to the public. In order to allow for potential future development of a transit stop, the proposed project includes the dedication of a "Future Railroad Platform". Additionally, the Center Avenue right-of-way will also be improved by the City of Huntington Beach Public Works as a capital project.

The Skate Park will be open to the public and operate from seven days a week from 10 am to 10 pm. The skate park will be supervised during business hours. The supervision will help to keep daily noise levels to a minimum. The skate park will also be a 100 percent fenced facility providing 24 hour security ensuring no additional noise after 10 pm.

The 3,500 square foot retail building will incorporate LEED design principals. The building will incorporate features such as skylights and large windows to maximize interior day lighting provided by the sun and reducing interior area lighting. It would also be constructed with recycled corrugated steel shipping containers to improve the sustainability of the project. The City of Huntington Beach, as part of the proposed skate park & retail building project, will reserve the right to install a transit platform on the property to implement a pedestrian transit stop in future. The transit reserve would consist of a 20 foot wide by 300 foot long area as illustrated on the Project Site Plan and will be clear of permanent building structures. The transit reservation will impact the parking and building placement, thus the parking lot may be redesigned to smaller standards further reducing storm water runoff and heat-island effect. This adjacent parking lot may create a shared use opportunity in the future with a potential city transit station.

The project will not be pursuing a LEED designation but will be designed using low-impact principles. California Skateparks will make every effort to balance soil on site avoiding trucking of material. The existing soils have high organic content which are not conducive for development of this type. California Skateparks will use sifting equipment on site to remove much of the organic content from the soil in order to reuse it and compacting it to the appropriate levels. The Staging Area at the north side by McFadden Avenue will be constructed with permeable gravel allowing storm water to promote clean infiltrate into the ground, then collected into underground pipes to avoid water runoff volume. The majority of impervious pavement will be the skate park areas and parking lot supporting the retail building. These areas will be planted with trees and shrubs to create shade that reduces the heat-island effect typical of urban development. Some plant beds and lawn areas may also function as a bio-swale to improve storm water quality. Site lighting will be designed with full cut-off fixtures and will eliminate light trespass to adjacent properties.



VANS will host up to 15 event days throughout the year, which would require the need for overflow parking and temporary seating areas for spectators. Twelve event days would be held on weekends and generally draw 300 to 500 spectators per event day, and event hours of operation would be 10 A.M. to 10 P.M., as under normal skate park operations. The remaining three event days would consist of one major event held annually expected to draw up to 2,500 spectators per event day, starting on a Friday and ending on a Sunday.

Temporary grandstand seating and portable restrooms will be placed to accommodate an audience of up to 2,500 people during and removed at the end of each event. The temporary grandstand seating for the events have been allocated within two options. Option 1 contains the bleachers set west of the VANS Leased property on Southern California Edison's property. Approximately 25'-0" wide by 446'-10" in length enclosed with a 6'-0" fence, CMU columns in between the fence lines, and all with a base of Decomposed Granite. All material will be grounded on SCE's property. Option 2 contains the bleachers set on the VANS Leased property on the North, Center, & South of the skate park location. No bleachers will be placed on the Southern California Edison or any type of improvements for Option 2.

During these events, visitors shall be directed to park their vehicles at the Huntington Beach Sports Complex (HBSC) located approximately 2.8 miles south of the project site, which has a total of 850 parking stalls. Guests would access this parking area via the HBSC's eastern entrance off Gothard Street at Talbert Avenue, and then would be transported to and from the skate park via shuttle buses; with up to six shuttle buses utilized to accommodate guest demand. Shuttle drivers would follow a specified shuttle route (i.e., Gothard Street between Center Avenue and Talbert Avenue). The shuttle buses would drop off / pick-up attendees at the drop off zone inside the Center Ave. turnaround driveway. Signage and/or parking attendants would be present to direct visitor vehicular traffic to the off-site parking area and direct pedestrians to the skate park area during such major events. Park visitors shall enter the park via the Center Avenue entrance and vendors shall enter the site through the McFadden Avenue entrance and station themselves in the north staging area. Prior to scheduling events, coordination with the City of Huntington Beach Community Services Department to allocate appropriate parking stall reserves at the HBSC. To ensure adequacy of parking, VANS would schedule major events on days where no events are planned at the HBSC.

The project will include a public address system used moderately during normal operation hours and no music will be played throughout the day. Amplified music and announcements will occur during special events and will continue through the duration of the events. A host will be on a loud speaker during this event.



Reduced Parking Accommodations - C.U.P.

Section 2.7.1-3 **Parking Regulation – Special Foods / Goods and Civic & Cultural** (Page 15) "Special Foods / Goods Spaces per 1,000 s.f. = 3 min / 4 max. & Civil & Cultural spaces per 1,000 s.f. = 3 min / 4 max."

C.U.P Requested: It is requested that a variance be granted to allow for 26 parking spaces within the property.

A C.U.P. is requested to allow for 26 parking spaces including 2 ADA stalls. 95 parking spaces are required. The property proposes only 26 parking spaces with the deficiency of 69 spaces. Because the BECSP does not specifically contain parking requirements for skate park land use, but the skate park is categorized as a civic and cultural use which does have the parking requirements. In order to deviate from the parking requirements of the civic and cultural use, a Variance & C.U.P. are required under the BECSP. The Huntington Beach Zoning Code does not state a parking requirement for skate park land use, but does specify that parking for park and recreation facilities would be as specified in this C.U.P. Because neither the BECSP nor the Zoning Code provides a parking requirement for skate park land use, the parking demand was determined through observations conducted at similar skate park facilities. The property is an approximately 2.7 acre with a unique configuration of 100' wide (92' excluding the Railroad Easement) by 1,190' long. This includes a skate park & retail building with a parking reduction of 26 parking spaces including 2 handicapped spaces. The parcel shape & size does not allow for 95 parking stalls with the additions of the skate park & retail building. The 27,000 skate park is proposed to be located in the center of the parcel to accommodate the water runoff requirements and precise grading & drainage purposes. One entry kiosk is also proposed by the skate park facing Center Avenue; therefore no main entrance will be on the McFadden Side. A 3,500 retail building is also located along Center Avenue with the 26 parking stalls serving in between the retail building & the skate park. With these and other site constraints such as setbacks & the railroad easement, there is no space to accommodate the BECSP civic and cultural parking requirements, therefore this request has been issued for the reduction of parking within the site. A study has been completed by a Licensed Traffic Engineer to determine that 26 stalls will suffice the demands of the typical days' parking count needs. Patrons of the skate park are expected to be youth who would typically utilize non-vehicular transportation such as transit, bikes and skateboards or be dropped off by others. Some patrons are expected to arrive by bus, as the project is located adjacent to the Golden West Transit Center. The Golden West Transit center is served by OCTA bus lines 29, 66, 70, 211 and 701. Additionally, the project proposes to set-aside a 20-foot-wide by 300-foot-long area for future installation of a transit platform, which will further facilitate non-vehicular travel to the site.



Joint Use Parking Accommodations & Distance of Joint Use Parking – VARIANCE / C.U.P.

VANS will host up to 15 event days throughout the year, which would require the need for overflow parking and temporary seating areas for spectators. Twelve event days would be held on weekends and generally draw 300 to 500 spectators per event day, and event hours of operation would be 10 A.M. to 10 P.M., as under normal skate park operations. The remaining three event days would consist of one major event held annually expected to draw up to 2,500 spectators per event day, starting on a Friday and ending on a Sunday. During these events, visitors would be directed to park their vehicles at the Huntington Beach Sports Complex (HBSC) which has a total of 850 parking spaces. Guests would access this parking area via the Sports Complex's eastern entrance off Gothard Street at Talbert Avenue.

Per Huntington Beach Zoning and Subdivision Ordinance (HBZSO) Section 231.06 the maximum distance between the building or use and the nearest point of the parking's spaces or parking facility shall be 250 feet. The HBSC is located approximately 2.8 miles from the project site. In order to deviate from the distance requirements for event days, a request for Variance & C.U.P. are required. To accommodate event guests, there would be transportation provided from the HBSC to the skate park via shuttle buses; with up to six shuttle buses utilized to accommodate guest demand. Shuttle drivers would follow a specified shuttle route (i.e., Gothard Street between Center Avenue and Talbert Avenue).

Signage and/or parking event attendants would also be present to direct event visitor vehicular traffic to the HBSC and direct event pedestrians to the skate park area during such major events. Park visitors would enter the park through the Center Avenue entrance. Shuttle buses would also enter the park through the Center Avenue entrance and drop off / pick up event visitors at the drop off only zone. Vendors would enter the site through the McFadden Avenue entrance and stage themselves in the north staging area.

Per HBZSO Section 231.06, there shall be no conflict in the operating hours based on parking space requirements for the different uses on the parcel. Prior to scheduling events, the project applicant would coordinate with the City of Huntington Beach Community Services Department to allocate appropriate parking stall reserves at the HBSC. To ensure adequacy of parking stall reserves, the project applicant would schedule major events on days where no events are planned at the HBSC.

A study has been completed by a Licensed Traffic Engineer to ensure that there are enough parking spaces at the sports complex to accommodate these events. Based on the above analysis, it is anticipated that the parking available at the HBSC will be sufficient to meet the demands of the project during special events. It is recommended that the project operators coordinate with the HBSC prior to organizing special events so that both facilities do not organize special events on the same day, this conclusion is in reference to the Special Events Parking Demand Analysis for this specific project.

VARIANCE: Per the Huntington Beach Zoning and Subdivision Ordinance (HBZSO) Section 231.06 the maximum distance between the building or use and the nearest point of the parking's spaces or parking facility shall be 250 feet.

Variance Requested: It is requested that a variance be granted to allow for overflow parking at the HBSC approximately 2.8 miles from the project site.



Driveway Width – VARIANCE

Section 2.7.3-1b. **General Parking Requirements – Curb Cuts & Driveways** (Page 68) "The maximum width of driveways/curb cuts is twelve (12) feet for a one-lane and twenty-four (24) feet for a two-lane driveway."

Variance Requested: It is requested that a variance be granted to allow for a twenty-six (26) foot driveway at the Center Avenue entrance to comply with HBFD and deviate from the twenty-four (24) foot driveway requirement per the BECSP.

1. A variance is requested to allow for a twenty-six (26) foot entry & driveway at Center Avenue to comply with HBFD and deviate from the twenty-four (24) foot driveway requirement per the BECSP. A variance to increase the BECSP requirement of a twenty-four (24) foot driveway to a twenty-six (26) foot driveway does not constitute a special privilege but clarification between both contradictory city requirements and also a precaution & priority to have emergency vehicles access the site with ease for a prompt response and ensure the public's safety. A variance for the twenty-six (26) foot driveway maintains all design, setback, screening, and other requirements while providing additional space for emergency vehicle access and also for public vehicle access. This deviation remains consistent with the Zoning Code's intent while allowing functional use of the property.

2. The unique property configuration of 100' wide (88' excluding the Transit Easement) by 1,190' long with a retail building at the frontage allows us only for one (1) entry and exit driveway. This entry way will be the prime and only access on Center Avenue but not limited to the public's vehicles, emergency vehicles, special event shuttles, waste management vehicles, etc... With this amount of site traffic, we feel to place priority on emergency vehicle access and comply with the HBFD requirements vs. BECSP requirements. A requirement of a 24'-0" maximum driveway per the BECSP as specified in Section 2.7.3-b would deprive the emergency vehicles access the site with ease for a prompt response and ensure the public's safety.

3. As an emergency vehicle, it is imperative that pedestrians & public vehicle traffic visually identify and emergency vehicle coming in to the proposed 26'-0" driveways, through the site, around the turnaround, and back to the exit if desired. Enough space for emergency vehicles should be a priority. A requirements of a 24'-0" driveway per the BECSP requirements would not meet the HBFD driveways requirements and will make it difficult for emergency vehicles to enter & exit with ease. A variance for the entry driveway width increase of two (2) feet on Center Avenue will maintain a design consistent with the Zoning Code's intent while allowing a much more functional use of entry/exit driveway.

4. Granting of the driveway width increase variance will not be materially detrimental to the public welfare. The purpose of the variance is, in fact, to provide an entry that will be more accessible to emergency vehicles and also the public. Said variance will not be injurious to other properties as the property is attempting to achieve similar accessibility as other properties and has no negative impact on the use of other properties.



Parking Lot Setback – VARIANCE

Section 2.7.3-1b. General Parking Requirements – Curb Cuts & Driveways (Page 68) “Driveways shall be set back a minimum of five feet from adjoining properties, and a minimum of three feet from adjacent buildings.”

Variance Requested: It is requested that a variance be granted for a reduction of three (3) feet from the five (5) foot minimum setback requirements per the BECSP from the adjacent Edison property just west of the site.

1. The need for a reduction of three (3) feet from the five (5) foot minimum setback requirements per the BECSP from the adjacent Edison property just west of the site is crucial; due to the narrow lot configuration of an approximate 92' of buildable space for the site. A variance for the setback reduction does not constitute a special privilege as no other properties in the vicinity have similar constraints. A variance for the setback reduction is due to a site constraint and with the variance granted, we will be able to stay consistent with the Zoning Code's intent while allowing functional use of the property.

2. The unique property configuration of 100' wide by 1,175' long and its constraints requires setback deviation to be located within the site for proper function. We currently have a 100' wide property, which is one of a couple if not the only site with these dimensions and restrictions. The property is between a railroad and an Edison property that is to construct OCTA parking. From the railroad, we have a fifteen (15) foot railroad easement encroaching into the property approximately eight (8) feet leaving our only 92'-0" of use for the property. From that easement line, we have placed a turnaround that includes a drop off zone and also complies with requirements for emergency vehicle. Emergency vehicles such as the fire department required to have a forty-five (45) foot inner radius and a seventeen (17) foot outer radius at turns. With these minimum requirements, the design of the turnaround is created at ninety-one (90) feet across; from back of curb at the railroad side easement line to back of curb perpendicular towards the Edison property. This leaves the property with a setback from the Edison Property of only 2'-0". A requirement of 5'-0" setback from the adjacent Edison property as specified in Section 2.7.3-1b would deprive the subject property of having good traffic flow and ease of access for emergency vehicles by restricting the design of a turnaround for both visibility from pedestrians and traffic but also restricting ease of access for emergency vehicles such as the fire department; thereby diminishing the function and value of the property.

3. As a business and a public amenity, it is imperative that pedestrians, public & emergency traffic visually identify the site's activities, functions, traffic flow, and emergency vehicle access. Alternate option designs per the Fire Department were considered but not recommended. Alternate design options such as the "L" & "T" turn for the subject property would not allow to have proper traffic direction & flow, in and out traffic would be dangerous for pedestrians within the property, difficulty of vehicles parking as they would be in each other's way, there would be no drop off zone for daily use of the skate park available therefore hindering vehicles to illegally stage themselves in fire lanes. In addition, no good flow would be available for shuttle buses to come in and out of the site with ease during special events causing a disruption to vehicles parking, emergency vehicles, and pedestrians. Therefore a requirement of a 5'-0" setback in this unique and restricted property would severely restrict all the items mentioned in this paragraph and would therefore substantially hinder the use, enjoyment, and commerce of the property, restricting the Owners property rights. A variance for a 2'-0" setback by the adjacent Edison property at the subject's property turnaround maintains traffic flow, fire department minimum requirements and will be consistent with the Zoning Codes' intent while allowing a great function use of the property.

4. Granting of the reduced setback at the adjacent Edison property variance will not be materially detrimental to the public welfare. The purpose of the variance is, in fact, to provide a project that will be more accessible to the public. Said variance will not be injurious to other properties as the subject property is attempting to achieve similar accessibility as other properties and has no negative impact on the use of other properties.



Parking Lot Landscape Screening – VARIANCE

Section 2.7.3-2b. **General Parking Requirements – Parking Lots** (Page 68) “Parking lots shall be buffered from adjacent development with moderate screening (see section 2.6.8)” Section 2.6.8-4b. **Open Space Landscaping – Walls and Fences** (Page 61) “side yards, defined as the portion of side setback areas behind the front setback area – and rear yards may contain landscape features that protect the privacy of the property’s occupants such as landscaping, trees and screening / wing walls.

Variance Requested: It is requested that a variance be granted to omit the landscape screening at the east side of the turnaround adjacent to the railroad easement.

1. The need to omit the landscape screening requirements per the BECSP from the adjacent railroad easement is due to the narrow lot configuration of an approximate 92’ of buildable space for the site. A variance to omit the landscape screening does not constitute a special privilege as no other properties in the vicinity have similar constraints. A variance to omit the landscape screening is due to a site constraint and with the variance granted, we will be able to stay consistent with the Zoning Code’s intent while allowing functional use of the property.

2. The unique property configuration of 100’ wide by 1,175’ long and its constraints requires setback deviation to be located within the site for proper function. We currently have a 100’ wide property, which is one of a couple if not the only site with these dimensions and restrictions. The property is between a railroad and an Edison property that is to construct OCTA parking. From the railroad, we have a fifteen (15) foot railroad easement encroaching into the property approximately eight (8) feet leaving our only 92’-0” of use for the property. From that easement line, we have placed a turnaround that includes a drop off zone and also complies with requirements for emergency vehicle. Emergency vehicles such as the fire department required to have a forty-five (45) foot inner radius and a seventeen (17) foot outer radius at turns. With these minimum requirements, the design of the turnaround is created at ninety-one (90) feet across; from back of curb at the railroad side easement line to back of curb perpendicular towards the Edison property. This leaves the property with a setback from the Edison Property of only 2’-0”. Since we propose to have a 2’-0” landscape buffer adjacent to the Edison property due to the fact that there is future development for OCTA parking, no space is available for a landscape buffer / screening on the opposite side of the site (adjacent to the Railroad) due to the fact that there is a 15’-0” Railroad Easement encroaching into the property. No development of any sort is allowed within the Rail road easement, not even landscaping. There currently is an existing fence that will remain at the railroad easement line within the property, this fence will abut the turnaround to comply with fire department requirements and provide good traffic flow within the site. All other areas will be screened, the area of about a 2’-0” to 3’-0” will not have landscape screening per the BECSP due to the property restrictions and it’s unique shape. A requirement to have landscape screening from adjacent properties as specified in Section 2.7.3-2b would deprive the subject property of having a turnaround to create good traffic flow and ease of access for emergency vehicles by restricting the design of a turnaround for both visibility from pedestrians and traffic but also restricting ease of access for emergency vehicles such as the fire department; thereby diminishing the function and value of the property.

3. As a business and a public amenity, it is imperative that pedestrians, public & emergency traffic visually identify the site’s activities, functions, traffic flow, and emergency vehicle access. Alternate design options such as the “L” & “T” turn for the subject property would allow for a landscape screening to adjacent properties but would be detrimental to proper traffic direction & flow, in and out traffic would be dangerous for pedestrians within the property, difficulty of vehicles parking as they would be in each other’s way, there would be no drop off zone for daily use of the skate park available therefore hindering vehicles to illegally stage themselves in fire lanes. In addition, no good flow would be available for shuttle buses to come in and out of the site with ease during special events causing a disruption to vehicles parking, emergency vehicles, and pedestrians. Therefore a requirement of landscape screening to adjacent properties in this unique and restricted property would severely restrict all the items mentioned in this paragraph and would therefore substantially hinder the use, enjoyment, and commerce of the property, restricting the Owners property rights. A variance to omit the landscape screening only at the turnaround adjacent to the railroad easement at the subject’s property turnaround maintains traffic flow, fire department minimum requirements and will be consistent with the Zoning Codes’ intent while allowing a great function use of the property.



Parking Lot Landscape Screening – VARIANCE Continued...

4. Granting of the reduced setback at the adjacent Edison property variance will not be materially detrimental to the public welfare. The purpose of the variance is, in fact, to provide a project that will be more accessible to the public. Said variance will not be injurious to other properties as the subject property is attempting to achieve similar accessibility as other properties and has no negative impact on the use of other properties.



Frontage Street Lighting – VARIANCE

Section 2.5.1-7. **Improvements to Existing Streets – Neighborhood Streets** (Page 50) "Streets with five (5) lanes or more shall provide pedestrian-scale/ boulevard-scale decorative street lighting at a maximum spacing of ninety (90) feet on-center. Pedestrian-scale light source should be located twelve to fourteen (12-14) feet above finished grade and boulevard-scale light sources should be located eighteen to twenty-five (18-25) feet above finished grade."

Variance Requested: It is requested that a variance be granted to omit a street light per due to the fact that a light already exists at the property's frontage.

1. The need for a street lighting variance is unique to this property due to the narrow lot configuration of 88' of buildable street frontage on Center Avenue. Due to the existing lighting in the public frontage, a variance to not install any new lighting along the Frontage of the property does not constitute a special privilege as no other properties in the vicinity have similar constraints. A variance for eliminating new lighting maintains a street façade consistent with the Zoning Code's intent while allowing functional use of the property.
2. The unique property configuration of 100' wide containing approximately 49' of frontage available for lighting (excluding the 20' future transit easement, 26' driveway and curbing entry, jogged sidewalk coming from the railroad side into the properties public frontage, and 5'-0" driveway setback from the adjacent Edison property) and existing street lighting along the property frontage, a street lighting variance is requested. This is a special circumstance applicable to this property as no other properties in the vicinity have similar constraints. A new proposed light would be only 40' from the existing light causing a "bright spot" on Center Ave. which would be detrimental to the uniform photometry along the roadway. The proposed light would also have a negative effect on the aesthetic appeal of the roadway by creating a non-uniform "cluster" of lights along the frontage. Currently, the existing street light spacing along Center Ave. is approximately 300'. The addition of a street light would be detrimental to the visibility of the proposed retail building frontage and also eliminate address identification to public and emergency vehicles. A variance for eliminating new lighting maintains a street façade consistent with the Zoning Code's intent while allowing functional use of the property.
3. The granting of the variance will preserve the enjoyment of substantial property rights. Adding an additional decorative street light in close proximity to the existing street light, proposed landscaping, and proposed water metering devices would cause an unappealing "clutter" along the frontage. This "clutter" could greatly reduce the visibility of the skate park and the retail building. As an emergency vehicle, address identification of all structures is priority for the safety of the public. A variance for eliminating new lighting maintains a street façade consistent with the Zoning Code's intent while allowing functional use of the property.
4. Granting the street lighting variance will not be materially detrimental to the public welfare since the existing street lighting will be maintained throughout the entire property frontage which is consistent with other properties in the area; as the property is attempting to achieve similar visibility as other properties and has no negative impact on the use of other properties.



FRONTAGE COVERAGE – VARIANCE

Section 2.4.7-2.a **Frontage Coverage – Regulation – General** (Page 43) "All developments shall include buildings located within the Frontage Coverage Zone as specified in Sec 2.1"

Variance Requested: The Frontage Coverage requirement of 90% is specified in Section 2.1. It is requested that a variance be granted to allow for a 50% Frontage Coverage based on the following rationale.

1. The need for a Frontage Coverage reduction variance is unique to this property due to the narrow lot configuration of 88' of buildable street frontage. A variance for 50% Frontage Coverage does not constitute a special privilege as no other properties in the vicinity have similar constraints. A variance for 50% Frontage Coverage maintains a street façade consistent with the Zoning Code's intent while allowing functional use of the property.
2. The unique property configuration of 100' wide (88' excluding the Transit Easement) by 1,175' long requires functions and improvements to be located a considerable distance from the street. On-site parking is located centrally on the site with a Fire Department required 26' wide access driveway. A requirement of 90% coverage as specified in Section 2.4.7-2.a would deprive the subject property of privileges enjoyed by other properties by restricting access both visually and physically to the property thereby diminishing the function and value of the property. This is a special circumstance applicable to this property as no other properties in the vicinity have similar constraints. A variance for 50% Frontage Coverage maintains a street façade consistent with the Zoning Code's intent while allowing functional use of the property.
3. As a business and a public amenity, it is imperative that pedestrians and vehicle traffic visually identify the site's activities and functions and physically have easy accessible access onto the site and into the on-site parking. A 90% Frontage Coverage would require a building length of 79' on an overall buildable frontage of 88', thus leaving only 9' open. A requirement of 90% coverage as specified in Section 2.4.7-2.a would severely restrict the visual and physical connections to the site and would therefor substantially hinder the use, enjoyment, and commerce of the property, restricting the Owner's property rights. A variance for 50% Frontage Coverage maintains a street façade consistent with the Zoning Code's intent while allowing functional use of the property.
4. Granting of the reduced Frontage Coverage variance will not be materially detrimental to the public welfare. The purpose of the variance is, in fact, to provide for a project that will be more accessible to the public. Said variance will not be injurious to other properties as the subject property is attempting to achieve similar accessibility as other properties and has no negative impact on the use of other properties.



Applicant/Owner

The project site is owned the by the City of Huntington Beach. Vans is leasing the site from the Agency and design, develop and plan operations at the proposed skate park.

Project Location

The proposed site is located on Center Avenue, approximately 500 feet east of Gothard Street. The project site is 2.718 acres in size and is identified as APN 142-073-03. A conceptual site plan is attached.

Planning Requirements

The Applicant is requesting approval of the following:

- Site Plan Review
- Conditional Use Permit
- Variance Application

Existing and Surrounding uses

The General Plan Map designates the parcel as Mix Use with both Specific Plan and Design Overlays (M-sp-d) and falls within the Beach and Edinger Corridors Specific Plan. The Specific Plan designates the site as Town Center – Neighborhood and requires residential.

The site is surrounded by following uses:

- West – Golden West College
- South – Commercial Strip Center/Bella Terra Phase II
- East – Old World Village/Multi-Family Residential
- North – Westminster Park/Industrial Use

Population Served

The skate park shall be open to the public and will serve local residents as well as city visitors. Generally speaking, the park shall be most frequented by youth, however, planned skate events are expected to attract visitors of all ages and from around the world.



Contacts

Applicant

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City of Huntington Beach Economic Development

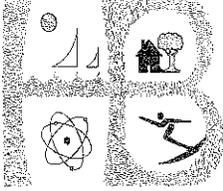
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City of Huntington Beach

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October 18, 2012

Joe Ciaglia
California Skateparks
273 N. Benson Avenue
Upland CA 91786

**SUBJECT: SITE PLAN REVIEW NO. 2012-003, CONDITIONAL USE PERMIT NO. 2012-009, VARIANCE NO. 2012-004 (SKATE PARK PROJECT)—7641 CENTER AVENUE
PROJECT IMPLEMENTATION CODE REQUIREMENTS**

Dear Mr. Ciaglia:

In order to assist you with your development proposal, staff has reviewed the project and identified applicable city policies, standard plans, and development and use requirements, excerpted from the Beach and Edinger Corridors Specific Plan (BECSP), City of Huntington Beach Zoning & Subdivision Ordinance (HBZSO) and Municipal Codes. This list is intended to help you through the permitting process and various stages of project implementation.

It should be noted that this requirement list is in addition to any "conditions of approval" adopted by the Planning Commission. Please note that if the design of your project or if site conditions change, the list may also change.

If you would like a clarification of any of these requirements, an explanation of the BECSP, HBZSO, and Municipal Codes, or believe some of the items listed do not apply to your project, and/or you would like to discuss them in further detail, please contact me at 714-374-1744 (tnguyen@surfcity-hb.org) and/or the respective source department (contact person below).

Sincerely,

TESS NGUYEN
Associate Planner

Enclosures

cc: Khoa Duong, Building & Safety – 714-872-6123
Dave Dominguez, Community Services – 714-374-5309
Jim Brown, Fire Department – 714-374-5344
Herb Fauland, Planning Manager

Steve Bogart, Public Works – 714-374-1692
Luis Gomez, Economic Development – 714-536-5544
Jason Kelly, Planning & Building Department
Project File

ATTACHMENT NO. 4.1



CITY OF HUNTINGTON BEACH

PROJECT IMPLEMENTATION CODE REQUIREMENTS

DATE: OCTOBER 2, 2012

PROJECT NAME: SKATE PARK PROJECT

PLANNING APPLICATION NO.: PLANNING APPLICATION NO. 12-067

ENTITLEMENTS: SITE PLAN REVIEW NO. 12-003
CONDITIONAL USE PERMIT NO. 12-009
VARIANCE NO. 12-004

DATE OF PLANS: SEPTEMBER 27, 2012

PROJECT LOCATION: 7461 CENTER AVENUE (APPROXIMATELY 500 FT. EAST OF GOTHARD STREET—APN 142-073-03)

PROJECT PLANNER: TESS NGUYEN, ASSOCIATE PLANNER

PLAN REVIEWER: KHOA DUONG, P.E

TELEPHONE/E-MAIL: (714) 872-6123/KHOA@CSGENGR.COM

PROJECT DESCRIPTION: TO ALLOW THE FOLLOWING:

- 1/ SITE PLAN REVIEW—DEVELOPMENT OF A SKATE PARK ON A 2.718-ACRE VACANT LOT, INCLUDING:
 - 14,500 SF SKATE PLAZA
 - 11,800 SF SKATE BOWL
 - 3,500 SF SKATE SHOP
 - 185 SF RESTROOM STRUCTURE
 - 150 SF SKATE PARK ENTRANCE KIOSK
 - 26 PARKING SPACES
- 2/ CONDITIONAL USE PERMIT
 - REDUCTION IN PARKING REQUIREMENTS
 - OFF-SITE PARKING AT THE SPORTS COMPLEX DURING SPECIAL EVENTS
- 3/ VARIANCE—DEVIATIONS IN THE FOLLOWING DEVELOPMENT STANDARDS:
 - DISTANCE OF JOINT USE PARKING DURING SPECIAL EVENTS
 - DRIVEWAY WIDTH
 - PARKING LOT SETBACKS
 - PARKING LOT LANDSCAPING SCREENING
 - FRONTAGE STREET LIGHTING
 - FRONTAGE COVERAGE

The following is a list of code requirements deemed applicable to the proposed project based on plans stated above. The list is intended to assist the applicant by identifying requirements which must be satisfied during the various stages of project permitting and implementation. A list of conditions of approval adopted by the Planning Commission in conjunction with the requested entitlement(s), if any, will also be provided upon final project approval. If you have any questions regarding these requirements, please contact the Plan Reviewer.

I. SPECIAL CONDITIONS:

1. None

II. CODE ISSUES BASED ON PLANS & DRAWINGS SUBMITTED:

1. Project shall comply with the current state building codes adopted by the City at the time of permit application submittal. Currently they are 2010 California Building Code (CBC), 2010 California Residential Code, 2010 California Mechanical Code (CMC), 2010 California Plumbing Code (CPC), 2010 California Electrical Code (CEC), 2010 California Energy Code, 2010 California Green Building Standards and The Huntington Beach Municipal Code (HBMC). Compliance to all applicable state and local codes is required prior to issuance of building permit.
2. Provide structural calculations for framing support the buildings.
3. Provide structural calculations for concrete retaining walls.
4. Provide energy calculations for building envelope, lighting system, and HVCA system.
5. Provide soil report.
6. The project must comply with Chapter 5 of the California Green Building Standards.
7. Building Code Summary shown on sheet A1.0, please specify the occupancy group A-5.
8. Please contact me or our office to review preliminary code analyses to examine any possible building code issue that may arise.

III. COMMENTS:

1. Planning and Building Department encourage the use of pre submittal zoning applications and building plan check meetings.
2. In addition to all of the code requirements of the 2010 California Green Building Standards Code, specifically provide a Construction Waste Management Plan per Sections 4.408.2 and 5.408.1.1.



CITY OF HUNTINGTON BEACH

PROJECT IMPLEMENTATION CODE REQUIREMENTS

DATE: OCTOBER 17, 2012

PROJECT NAME: SKATE PARK PROJECT

PLANNING APPLICATION NO.: PLANNING APPLICATION NO. 12-067

ENTITLEMENTS: SITE PLAN REVIEW NO. 12-003
CONDITIONAL USE PERMIT NO. 12-009
VARIANCE NO. 12-004

DATE OF PLANS: SEPTEMBER 27, 2012

PROJECT LOCATION: 7461 CENTER AVENUE (APPROXIMATELY 500 FT. EAST OF
GOTHARD STREET—APN 142-073-03)

PROJECT PLANNER: TESS NGUYEN, ASSOCIATE PLANNER

PLAN REVIEWER: David Dominguez

TELEPHONE/E-MAIL: (714) 374-5309/ ddominguez@surfcity-hb.org

PROJECT DESCRIPTION: TO ALLOW THE FOLLOWING:

- 1/ SITE PLAN REVIEW—DEVELOPMENT OF A SKATE PARK ON A 2.718-ACRE VACANT LOT, INCLUDING:
 - 14,500 SF SKATE PLAZA
 - 11,800 SF SKATE BOWL
 - 3,500 SF SKATE SHOP
 - 185 SF RESTROOM STRUCTURE
 - 150 SF SKATE PARK ENTRANCE KIOSK
 - 26 PARKING SPACES
- 2/ CONDITIONAL USE PERMIT
 - REDUCTION IN PARKING REQUIREMENTS
 - OFF-SITE PARKING AT THE SPORTS COMPLEX DURING SPECIAL EVENTS
- 3/ VARIANCE—DEVIATIONS IN THE FOLLOWING DEVELOPMENT STANDARDS:
 - DISTANCE OF JOINT USE PARKING DURING SPECIAL EVENTS
 - DRIVEWAY WIDTH
 - PARKING LOT SETBACKS
 - PARKING LOT LANDSCAPING SCREENING
 - FRONTAGE STREET LIGHTING
 - FRONTAGE COVERAGE

The following is a list of code requirements deemed applicable to the proposed project based on plans stated above. The list is intended to assist the applicant by identifying requirements which must be satisfied during the various stages of project permitting and implementation. A list of conditions of approval adopted by the Planning Commission in conjunction with the requested entitlement(s), if any, will also be provided upon final project approval. If you have any questions regarding these requirements, please contact the Plan Reviewer.

Community Services has no code requirements for the proposed project.



CITY OF HUNTINGTON BEACH

PROJECT IMPLEMENTATION CODE REQUIREMENTS

DATE: OCTOBER 4, 2012

PROJECT NAME: SKATE PARK PROJECT

PLANNING APPLICATION NO.: PLANNING APPLICATION NO. 12-067

ENTITLEMENTS: SITE PLAN REVIEW NO. 12-003
CONDITIONAL USE PERMIT NO. 12-009
VARIANCE NO. 12-004

DATE OF PLANS: SEPTEMBER 27, 2012

PROJECT LOCATION: 7461 CENTER AVENUE (APPROXIMATELY 500 FT. EAST OF
GOTHARD STREET—APN 142-073-03)

PROJECT PLANNER: TESS NGUYEN, ASSOCIATE PLANNER

PLAN REVIEWER: LUIS GOMEZ, ECONOMIC DEVELOPMENT

TELEPHONE/E-MAIL: (714) 536-5544/ luis.gomez@surfcity-hb.org

PROJECT DESCRIPTION: TO ALLOW THE FOLLOWING:

- 1/ SITE PLAN REVIEW—DEVELOPMENT OF A SKATE PARK ON A 2.718-ACRE VACANT LOT, INCLUDING:
 - 14,500 SF SKATE PLAZA
 - 11,800 SF SKATE BOWL
 - 3,500 SF SKATE SHOP
 - 185 SF RESTROOM STRUCTURE
 - 150 SF SKATE PARK ENTRANCE KIOSK
 - 26 PARKING SPACES
- 2/ CONDITIONAL USE PERMIT
 - REDUCTION IN PARKING REQUIREMENTS
 - OFF-SITE PARKING AT THE SPORTS COMPLEX DURING SPECIAL EVENTS
- 3/ VARIANCE—DEVIATIONS IN THE FOLLOWING DEVELOPMENT STANDARDS:
 - DISTANCE OF JOINT USE PARKING DURING SPECIAL EVENTS
 - DRIVEWAY WIDTH
 - PARKING LOT SETBACKS
 - PARKING LOT LANDSCAPING SCREENING
 - FRONTAGE STREET LIGHTING
 - FRONTAGE COVERAGE

The following is a list of code requirements deemed applicable to the proposed project based on plans stated above. The list is intended to assist the applicant by identifying requirements which must be satisfied during the various stages of project permitting and implementation. A list of conditions of approval adopted by the Planning Commission in conjunction with the requested entitlement(s), if any, will also be provided upon final project approval. If you have any questions regarding these requirements, please contact the Plan Reviewer.

Economic Development has reviewed the Planning application for the Vans Skate Park and has the following comments:

1. The skate park shall maintain a 20' x 300' transit reserve area located on south side of the project site for future transit purposes.
2. The skate park shall remain free of charge and open to the public, with the exception of 15 event days per year.



HUNTINGTON BEACH FIRE DEPARTMENT PROJECT IMPLEMENTATION CODE REQUIREMENTS

RECEIVED

OCT 03 2012

Dept. of Planning
& Building

DATE: OCTOBER 3, 2012

PROJECT NAME: SKATE PARK

ENTITLEMENTS: PLANNING APPLICATION NO. 12-067

PROJECT LOCATION: 7461 CENTER AVENUE
(NORTH SIDE OF CENTER AVENUE, EAST OF GOTHARD STREET)

ENTITLEMENTS: SITE PLAN REVIEW NO. 12-003
CONDITIONAL USE PERMIT NO. 12-009
VARIANCE NO. 12-004

PLANNER: TESS NGUYEN, ASSOCIATE PLANNER

TELEPHONE/E-MAIL: (714) 374-1744/ tnguyen@surfcity-hb.org

PLAN REVIEWER-FIRE: JAMES BROWN, FIRE PROTECTION ANALYST

TELEPHONE/E-MAIL: (714) 374-5344/ jbrown@surfcity-hb.org

PROJECT DESCRIPTION: TO ALLOW THE FOLLOWING:

- 1/ SITE PLAN REVIEW—DEVELOPMENT OF A SKATE PARK ON A 2.718-ACRE VACANT LOT, INCLUDING:
 - 14,500 SF SKATE PLAZA
 - 11,800 SF SKATE BOWL
 - 3,500 SF SKATE SHOP
 - 185 SF RESTROOM STRUCTURE
 - 150 SF SKATE PARK ENTRANCE KIOSK
 - 26 PARKING SPACES
- 2/ CONDITIONAL USE PERMIT
 - REDUCTION IN PARKING REQUIREMENTS
 - OFF-SITE PARKING AT THE SPORTS COMPLEX DURING SPECIAL EVENTS
- 3/ VARIANCE—DEVIATIONS IN THE FOLLOWING DEVELOPMENT STANDARDS:
 - DISTANCE OF JOINT USE PARKING DURING SPECIAL EVENTS
 - DRIVEWAY WIDTH
 - PARKING LOT SETBACKS
 - PARKING LOT LANDSCAPING SCREENING
 - FRONTAGE STREET LIGHTING
 - FRONTAGE COVERAGE

The following is a list of code requirements deemed applicable to the proposed project based on plans received and dated SEPTEMBER 27, 2012. The list is intended to assist the applicant by identifying requirements which must be satisfied during the various stages of project permitting and implementation. A list of conditions of approval adopted by the Planning Commission in conjunction with the requested entitlement(s), if any, will also be provided upon final project approval. If you have any questions

ATTACHMENT NO. 4.8

regarding these requirements, please contact the Plan Reviewer- Fire: JAMES BROWN, FIRE PROTECTION ANALYST.

PRIOR TO DEMOLITION, GRADING, SITE DEVELOPMENT, ISSUANCE OF GRADING PERMITS, BUILDING PERMITS, AND/OR CONSTRUCTION, THE FOLLOWING SHALL BE REQUIRED:

Environmental

- a. ***City Specification # 431-92 Soil Clean-Up Standards testing is required.*** Based on site characteristics, suspected soil contamination, proximity to a producing/abandoned oil well, or Phase I, II, or III Site Audit, soil testing conforming to City Specification # 431-92 Soil Clean-Up Standards is required.

All soils shall conform to City Specification # 431-92 Soil Clean-Up Standards prior to the issuance of a building permit. Building plans shall reference that "All soils shall conform to City Specification # 431-92 Soil Clean-Up Standards" in the plan notes.

Note: Grading Plans must be approved by the Fire Department prior to issuance of a Public Works grading permit. Standard Fire Department notes are required to be on the plans on potentially impacted sites.

Soil testing results must be submitted, and approved by the Fire Department prior to issuance of a building permit. (FD)

- b. **Environmental - Elevated levels of methane or other soil gases in the area. (No well)**

Methane Mitigation District Requirements. The proposed construction is within the City of Huntington Beach Methane Mitigation District. Due to known elevated levels of methane or other soil gases in this area, a sub-slab methane barrier and vent system is required for this project.

NOTE: The Phase 1 ESA has indicated that there is a layer of peat in the soil. Peat typically produces elevated levels of methane. As such, there might be a requirement for a methane barrier under the buildings.

The following City Specification is applicable and the grading, building, and methane plans must reference that a sub-slab methane barrier and vent system will be installed per City Specification # 429, *Methane District Building Permit Requirements* prior to plan approval.

Methane safety measures per *City Specification # 429, Methane District Building Permit Requirements* shall be detailed on a separate sheet titled "METHANE PLAN" and three copies submitted to the Fire Department for approval. (FD)

- c. **"Remediation Action Plan"** If contamination is identified, provide a Fire Department approved Remediation Action Plan (RAP) based on requirements found in Huntington Beach City Specification #431-92, *Soil Cleanup Standard*. Upon remediation action plan approval, a rough grading permit may be issued. (FD)
- d. **Discovery of soil contamination/pipelines**, etc., must be reported to the Fire Department immediately and an approved remedial work plan submitted. (FD)

Fire Master Plan

A separate Fire Master Plan is required for submittal to the HBFD. It shall be a site plan reflecting all the following fire department related items:

- Fire hydrant locations, public and private.
- FDC locations.
- Dimensions from FDC's to hydrants.
- DCDA locations.
- Fire sprinkler riser locations.
- FACP locations.
- Knox box and knox switch locations.
- Gate locations, if required.
- Fire lane locations, dimensions, lengths, turning radii at corners and circles/cul-de-sacs.
- Fire lane signage and striping.
- Property dimensions or accurate scale.
- Building locations and heights.
- Building addresses and suite addresses. (FD)

Fire Suppression Systems

Fire Protection Systems

Fire Extinguishers shall be installed and located in all areas to comply with California Fire Code standards found in *City Specification #424*. The minimum required dry chemical fire extinguisher size is 2A 10BC and shall be installed within 75 feet travel distance to all portions of the building. Extinguishers are required to be serviced or replaced annually. (FD)

Commercial Food Preparation Fire Protection System required for commercial cooking. Plans (three sets) shall be submitted to the Fire Department as separate plans for permits and approval. Reference compliance with *City Specification # 412 Protection Of Commercial Cooking Operations* in the plan notes. (FD)

Fire Apparatus Access

Fire Access Road Turns and Corners shall be designed with a minimum inner radius of seventeen feet (17') and a minimum outer radius of forty five feet (45') per *City Specification # 401 Minimum Standards for Fire Apparatus Access*. For Fire Department approval, reference and demonstrate compliance with *City Specification # 401 Minimum Standards for Fire Apparatus Access* on the plans. (FD)

Fire Lanes and Fire Access Roads shall be marked and signed as per the CFC, HBMC and *City Specification # 415, Fire Lanes Signage and Markings on Private, Residential, commercial and Industrial Properties*. For Fire Department approval, reference and demonstrate compliance on the plans. (FD)

Fire Personnel Access

Main Secured Building Entries shall utilize a KNOX® Fire Department Access Key Box, installed and in compliance with *City Specification #403, Fire Access for Pedestrian or Vehicular Security Gates & Buildings*. Please contact the Huntington Beach Fire Department Administrative Office at (714) 536-5411 for information. Reference compliance with *City Specification #403 - KNOX® Fire Department Access* in the building plan notes. (FD)

Addressing and Street Names

Commercial Building Address Numbers shall be installed to comply with *City Specification #428, Premise Identification*. Building address number sets are required on front and rear of the structure and shall be a minimum of six inches (6") high with one and one half inch (1 ½") brush stroke. Note: Units shall be identified with numbers per *City Specification # 409 Street Naming and Address Assignment Process*. Unit address numbers shall be a minimum of four inches (4") affixed to the units front and rear door. All address numbers are to be in a contrasting color. For Fire Department approval, reference compliance with *City Specification #428 Premise Identification* in the plan notes and reflect the address location on the building. (FD)

GIS Mapping Information

GIS Mapping Information shall be provided to the Fire Department in compliance with GIS Department CAD Submittal Guideline requirements. Minimum submittals shall include the following:

- Site plot plan showing the building footprint.
- Specify the type of use for the building
- Location of electrical, gas, water, sprinkler system shut-offs.
- Fire Sprinkler Connections (FDC) if any.
- Knox Access locations for doors, gates, and vehicle access.
- Street name and address.

Final site plot plan shall be submitted in the following digital format and shall include the following:

- Submittal media shall be via CD rom to the Fire Department.
- Shall be in accordance with County of Orange Ordinance 3809.
- File format shall be in .shp, AutoCAD, AUTOCAD MAP (latest possible release) drawing file - .DWG (preferred) or Drawing Interchange File - .DXF.
- Data should be in NAD83 State Plane, Zone 6, Feet Lambert Conformal Conic Projection.
- Separate drawing file for each individual sheet.
In compliance with Huntington Beach Standard Sheets, drawing names, pen colors, and layering convention. and conform to *City of Huntington Beach Specification # 409 – Street Naming and Addressing*.

For specific GIS technical requirements, contact the Huntington Beach GIS Department at (714) 536-5574.

For Fire Department approval, reference compliance with *GIS Mapping Information* in the building plan notes. **(FD)**

Building Construction

Exit Signs And Exit Path Markings will be provided in compliance with the Huntington Beach Fire Code and Title 24 of the California Administrative Code. Reference compliance in the plan notes. **(FD)**

THE FOLLOWING CONDITIONS SHALL BE MAINTAINED DURING CONSTRUCTION:

- a. Fire/Emergency Access And Site Safety shall be maintained during project construction phases in compliance with HBFC Chapter 14, Fire Safety During Construction And Demolition. **(FD)**
- b. Fire/Emergency Access And Site Safety shall be maintained during project construction phases in compliance with City Specification #426, Fire Safety Requirements for Construction Sites. **(FD)**

OTHER:

- a. Discovery of additional soil contamination or underground pipelines, etc., must be reported to the Fire Department immediately and the approved work plan modified accordingly in compliance with City Specification #431-92 Soil Clean-Up Standards. **(FD)**

- b. Outside City Consultants: The Fire Department review of this project and subsequent plans may require the use of City consultants. The Huntington Beach City Council approved fee schedule allows the Fire Department to recover consultant fees from the applicant, developer or other responsible party. **(FD)**

Fire Department City Specifications may be obtained at:

Huntington Beach Fire Department Administrative Office

City Hall 2000 Main Street, 5th floor

Huntington Beach, CA 92648

or through the City's website at

http://www.huntingtonbeachca.gov/government/departments/Fire/fire_prevention_code_enforcement/fire_dept_city_specifications.cfm

If you have any questions, please contact the Fire Prevention Division at (714) 536-5411.



**HUNTINGTON BEACH
PLANNING AND BUILDING DEPARTMENT
PROJECT IMPLEMENTATION CODE REQUIREMENTS**

DATE: OCTOBER 18, 2012

PROJECT NAME: SKATE PARK PROJECT

PLANNING APPLICATION NO.: PLANNING APPLICATION NO. 12-067

ENTITLEMENTS: SITE PLAN REVIEW NO. 12-003
CONDITIONAL USE PERMIT NO. 12-009
VARIANCE NO. 12-004

DATE OF PLANS: SEPTEMBER 27, 2012

PROJECT LOCATION: 7461 CENTER AVENUE (APPROXIMATELY 500 FT. EAST OF
GOTHARD STREET—APN 142-073-03)

PLAN REVIEWER: TESS NGUYEN, ASSOCIATE PLANNER

TELEPHONE/E-MAIL: (714) 374-1744/ tnguyen@surfcity-hb.org

PROJECT DESCRIPTION: SITE PLAN REVIEW—TO ALLOW THE DEVELOPMENT OF A SKATE PARK ON A 2.718-ACRE VACANT LOT, INCLUDING A 14,500 SF SKATE PLAZA, A 11,800 SF SKATE BOWL, A 3,500 SF SKATE SHOP, A 185 SF RESTROOM STRUCTURE, A 150 SF SKATE PARK ENTRANCE KIOSK, AND 26 PARKING SPACES
CONDITIONAL USE PERMIT—TO ALLOW THE REDUCTION IN PARKING REQUIREMENTS AND OFF-SITE PARKING AT THE SPORTS COMPLEX DURING SPECIAL EVENTS
VARIANCE—TO PERMIT DEVIATIONS IN THE FOLLOWING DEVELOPMENT STANDARDS: DISTANCE OF JOINT USE PARKING DURING SPECIAL EVENTS, DRIVEWAY WIDTH, PARKING LOT SETBACKS, PARKING LOT LANDSCAPING SCREENING, FRONTAGE STREET LIGHTING, FRONTAGE COVERAGE

The following is a list of code requirements deemed applicable to the proposed project based on plans received and dated September 27, 2012. The list is intended to assist the applicant by identifying requirements which must be satisfied during the various stages of project permitting and implementation. A list of conditions of approval adopted by the Planning Commission in conjunction with the requested entitlement(s), if any, will also be provided upon final project approval. If you have any questions regarding these requirements, please contact the Plan Reviewer.

SITE PLAN REVIEW NO. 12-003/CONDITIONAL USE PERMIT NO. 12-009/ VARIANCE NO. 12-004:

1. The site plan, floor plans, and elevations approved by the Planning Commission shall be the conceptually approved design with modifications as specified below.
2. Prior to issuance of grading permits, the following shall be completed:
 - a. A Landscape and Irrigation Plan, prepared by a Licensed Landscape Architect shall be submitted to the Planning and Building Department for review and approval. **(HBZSO Section 232.04)**
 - b. "Smart irrigation controllers" and/or other innovative means to reduce the quantity of runoff shall be installed. **(HBZSO Section 232.04.D)**
 - c. All landscape planting, irrigation and maintenance shall comply with the City Arboricultural and Landscape Standards and Specifications. **(HBZSO Section 232.04.B)**
 - d. Standard landscape code requirements apply. **(HBZSO Chapter 232)**
 - e. Landscaping plans should utilize native, drought-tolerant landscape materials where appropriate and feasible. **(HBZSO Section 232.06.A)**
 - f. The Consulting Arborist (approved by the City Landscape Architect) shall review the final landscape tree planting plan and approve in writing the selection and locations proposed for new trees. Said Arborist report shall be incorporated onto the Landscape Architect's plans as construction notes and/or construction requirements. The report shall include the Arborist's name, certificate number and the Arborist's wet signature on the final plan. **(Resolution No. 4545)**
3. A minimum of 14 days prior to submittal for building permits, an application for address assignment, along with the corresponding application processing fee and applicable plans (as specified in the address assignment application form), shall be submitted to the Planning Department. **(City Specification No. 409)**
4. Prior to issuance of building permits, the following shall be completed:
 - a. Joint use parking shall require a Joint Use Parking Agreement between property owners to be recorded prior to issuance of permits or occupancy. The legal instrument shall be submitted to the Planning and Building Department a minimum of 30 days prior to building permit issuance. A copy of the legal instrument shall be approved by the City Attorney as to form and content and, when approved, shall be recorded in the Office of the County Recorder. A copy of the recorded agreement shall be filed with the Planning and Building Department. The recorded agreement shall remain in effect in perpetuity, except as modified or rescinded pursuant to the expressed written approval of the City of Huntington Beach. **(HBZSO Section 231.06)**
 - b. The Beach and Edinger Corridors Specific Plan fee shall be paid. The fee is \$0.30 per square foot of net new office and commercial development. **(Resolution No. 2010-80)**

- c. A General Plan Maintenance Fee shall be paid. The fee is \$1.85/\$1,000 valuation of new construction. **(City of Huntington Beach Planning Division Fee Schedule)**
- d. A Mitigation Monitoring Fee for the Environmental Impact Report shall be paid to the Planning and Building Department pursuant to the fee schedule adopted by resolution of the City Council. **(City of Huntington Beach Planning Division Fee Schedule)**
5. During demolition, grading, site development, and/or construction, all Huntington Beach Zoning and Subdivision Ordinance and Municipal Code requirements including the Noise Ordinance shall be adhered to. All activities including truck deliveries associated with construction, grading, remodeling, or repair shall be limited to Monday - Saturday 7:00 AM to 8:00 PM. Such activities are prohibited Sundays and Federal holidays. **(HBMC 8.40.090)**
6. The structure(s) cannot be occupied, the final building permit(s) cannot be approved, and commencement of use and issuance of a Certificate of Occupancy) until the following has been completed:
 - a. A Certificate of Occupancy must be approved by the Planning Division and issued by the Building Division. **(HBMC 17.04.036)**
 - b. Complete all improvements as shown on the approved grading, landscape and improvement plans. **(HBMC 17.05)**
 - c. All trees shall be maintained or planted in accordance to the requirements of Chapter 232. **(HBZSO Chapter 232)**
 - d. All landscape irrigation and planting installation shall be certified to be in conformance to the City approved landscape plans by the Landscape Architect of record in written form to the City Landscape Architect. **(HBZSO Section 232.04.D)**
 - e. The provisions of the Water Efficient Landscape Requirements shall be implemented. **(HBMC 14.52)**
 - f. Plans for all signage shall be submitted to the Planning Division. Said signage shall be approved prior to the issuance of the first sign permit. **(BECSP Section 2.9 and HBZSO Section 233.04)**
7. The Development Services Departments (Building & Safety, Fire, Planning and Public Works) shall be responsible for ensuring compliance with all applicable code requirements and conditions of approval. The Director of Planning may approve minor amendments to plans and/or conditions of approval as appropriate based on changed circumstances, new information or other relevant factors. Any proposed plan/project revisions shall be called out on the plan sets submitted for building permits. Permits shall not be issued until the Development Services Departments have reviewed and approved the proposed changes for conformance with the intent of the Planning Commission's /Zoning Administrator's action. If the proposed changes are of a substantial nature, an amendment to the original entitlement reviewed by the Planning Commission /Zoning Administrator may be required pursuant to the provisions of HBZSO Section 241.18. **(HBZSO Section 241.18)**
8. Site Plan Review No. 12-003, Conditional Use Permit No. 12-009, and Variance No. 12-004 shall become null and void unless exercised within one year of the date of final approval or

such extension of time (maximum of two years) as may be granted by the Director pursuant to a written request submitted to the Planning and Building a minimum of 30 days prior to the expiration date. **(BECSP Section 2.0.5)**

9. Site Plan Review No. 12-003, Conditional Use Permit No. 12-009, and Variance No. 12-004 shall not become effective until the appeal period following the approval of the entitlements has elapsed. **(BECSP Section 2.0.5)**
10. The project shall comply with all applicable requirements of the Municipal Code, Building & Safety Department and Fire Department, as well as applicable local, State and Federal Fire Codes, Ordinances, and standards, except as noted herein. **(City Charter, Article V)**
11. The applicant shall submit a check in the amount of \$50.00 for the posting of the Notice of Determination at the County of Orange Clerk's Office. The check shall be made out to the County of Orange and submitted to the Planning Department within two (2) days of the Planning Commission's action. **(California Code Section 15094 and Fish and Game Section 711.4)**
12. All landscaping shall be maintained in a neat and clean manner, and in conformance with the HBZSO. Prior to removing or replacing any landscaped areas, check with the Departments of Planning and Public Works for Code requirements. Substantial changes may require approval by the Planning Commission. **(HBZSO Section 232.04)**
13. All permanent, temporary, or promotional signs shall conform to Chapter 233 of the HBZSO and BECSP. Prior to installing any new signs, changing sign faces, or installing promotional signs, applicable permit(s) shall be obtained from the Planning Division. Violations of this ordinance requirement may result in permit revocation, recovery of code enforcement costs, and removal of installed signs. **(BECSP 2.9 Sign Regulations)**

BECSP ADDITIONAL CODE REQUIREMENTS AND PLAN COMPLIANCE

1. All code requirements are specified as development "standards" or "regulations" in the BECSP. Compliance with all standards and regulations is mandatory. Any deviations from the development standards must be requested to the Director and shall not deviate more than 10 percent from any single standard. Deviations greater than 10 percent shall require submittal of a variance application subject to approval from the Planning Commission. **(Section 2.0)**
2. The project shall comply with all applicable standards of the Town Center Neighborhood Segment. **(Section 2.1.4)**
3. The primary volume of the retail building shall be defined with a major façade offset, notch, and façade composition change. **(Section 2.3.5)**
4. The shopfront of the retail building shall contain a minimum of 70% of the storefront façade featuring clear-glass display windows framed within storefront pilasters and base. **(Section 2.4.2)**
5. A substantial horizontal articulation of the street façade of the retail building shall be applied at the top of the uppermost floor of the façade, to result in a termination of the façade that provides an attractive façade skyline and a completion of the upper façade composition. A

horizontal articulation of the street façade shall be applied within the first floor to form a horizontal "base" of the façade at the building scale. **(Section 2.8.1)**

6. Wall composition for street façade of the retail building shall contain a minimum of twenty (20) percent and a maximum of sixty (60) percent glazing (percentage does not include parapet height). **(Section 2.8.2)**



CITY OF HUNTINGTON BEACH

PUBLIC WORKS INTERDEPARTMENTAL COMMUNICATION

PROJECT IMPLEMENTATION CODE REQUIREMENTS

DATE: OCTOBER 4, 2012

PROJECT NAME: VAN'S SKATE PARK

ENTITLEMENTS: CUP 12-09, SPR 12-03, VAR 12-04

PLNG APPLICATION NO: 2012-0067

DATE OF PLANS: SEPTEMBER 27, 2012

PROJECT LOCATION: 7461 CENTER AVENUE, APPROXIMATELY 500 FT. EAST OF
GOTHARD STREET (APN 142-073-03)

PROJECT PLANNER: TESS NGUYEN, ASSOCIATE PLANNER

TELEPHONE/E-MAIL: 714-374-1744 / TNGUYEN@SURFCITY-HB.ORG

PLAN REVIEWER: STEVE BOGART, SENIOR CIVIL ENGINEER

TELEPHONE/E-MAIL: 714-374-1692 / SBOGART @SURFCITY-HB.ORG

PROJECT DESCRIPTION: TO ALLOW THE FOLLOWING:

- 1/ SITE PLAN REVIEW—DEVELOPMENT OF A SKATE PARK ON A
2.718-ACRE VACANT LOT, INCLUDING:
 - 14,500 SF SKATE PLAZA
 - 11,800 SF SKATE BOWL
 - 3,500 SF SKATE SHOP
 - 185 SF RESTROOM STRUCTURE
 - 150 SF SKATE PARK ENTRANCE KIOSK
 - 26 PARKING SPACES
- 2/ CONDITIONAL USE PERMIT
 - REDUCTION IN PARKING REQUIREMENTS
 - OFF-SITE PARKING AT THE SPORTS COMPLEX DURING
SPECIAL EVENTS
- 3/ VARIANCE—DEVIATIONS IN THE FOLLOWING DEVELOPMENT
STANDARDS:
 - DISTANCE OF JOINT USE PARKING DURING SPECIAL
EVENTS
 - DRIVEWAY WIDTH
 - PARKING LOT SETBACKS
 - PARKING LOT LANDSCAPING SCREENING
 - FRONTAGE STREET LIGHTING
 - FRONTAGE COVERAGE

The following is a list of code requirements deemed applicable to the proposed project based on plans as stated above. The items below are to meet the City of Huntington Beach's Municipal Code (HBMC), Zoning and Subdivision Ordinance (ZSO), Department of Public Works Standard Plans (Civil, Water and Landscaping) and the American Public Works Association (APWA) Standards Specifications for Public Works Construction (Green Book), the Orange County Drainage Area management Plan (DAMP), and the City Arboricultural and Landscape Standards and Specifications. The list is intended to assist the applicant by identifying requirements which shall be satisfied during the various stages of project permitting, implementation and construction. If you have any questions regarding these requirements, please contact the Plan Reviewer or Project Planner.

**THE FOLLOWING DEVELOPMENT REQUIREMENTS SHALL BE COMPLETED PRIOR TO
ISSUANCE OF A GRADING PERMIT:**

1. A Legal Description and Plot Plan of the dedications to City to be prepared by a licensed surveyor or engineer and submitted to Public Works for review and approval. The dedication shall be recorded prior to issuance of a grading permit.
2. The following dedications to the City of Huntington Beach shall be shown on the Precise Grading Plan. (ZSO 230.084A)
 - a. A four (4)-foot right-of-way dedication for pedestrian access and public utilities along the Center Avenue frontage is required, per Public Works Standard Plan Nos. 104 and 207.
3. A Precise Grading Plan, prepared by a Licensed Civil Engineer, shall be submitted to the Public Works Department for review and approval. (MC 17.05/ZSO 230.84) The plans shall comply with Public Works plan preparation guidelines and include the following improvements on the plan:
 - a. The proposed driveway approach on Center Avenue shall be constructed per Public Works Standard Plan No. 211. The minimum width of the driveway approach shall be 26 feet per Standard Plan No. 211. (ZSO 230.84)
 - b. The proposed driveway approach on McFadden Avenue shall be constructed per Public Works Standard Plan No. 211. The minimum width of the driveway approach shall be 26 feet per Standard Plan No. 211. (ZSO 230.84)
 - c. The project's Center Avenue frontage (including but not limited to curb, gutter, sidewalk, parkway, street trees and street lights) shall be constructed per Public Works Standard Plan Nos. 202 and 207 and per the Neighborhood Street cross section (of the Town Center – Neighborhood segment) configuration of the Beach and Edinger Corridors Specific Plan (BECSP). This BECSP configuration provides six (6) feet of landscaped parkway at the back of curb and a six (6) foot-wide sidewalk. (ZSO 230.84, BECSP)
 - d. A new sewer lateral shall be constructed to serve the subject site. (ZSO 230.84)
 - e. A new domestic water service and meter shall be installed per Water Division Standards, and sized to meet the minimum requirements set by the California Plumbing Code (CPC). (ZSO 255.04, MC 14.08.020)
 - f. A separate irrigation water service and meter shall be installed per Water Division Standards. (ZSO 232)

- h. Describes the mechanism for funding the long-term operation and maintenance of the Treatment Control BMPs.
 - i. Includes an Operations and Maintenance (O&M) Plan for all structural BMPs.
 - j. After incorporating plan check comments of Public Works, three final WQMPs (signed by the owner and the Registered Civil Engineer of record) shall be submitted to Public Works for acceptance. After acceptance, two copies of the final report shall be returned to applicant for the production of a single complete electronic copy of the accepted version of the WQMP on CD media that includes:
 - i. The 11" by 17" Site Plan in .TIFF format (400 by 400 dpi minimum).
 - ii. The remainder of the complete WQMP in .PDF format including the signed and stamped title sheet, owner's certification sheet, Inspection/Maintenance Responsibility sheet, appendices, attachments and all educational material.
 - k. The applicant shall return one CD media to Public Works for the project record file.
8. Indicate the type and location of Water Quality Treatment Control Best Management Practices (BMPs) on the Grading Plan consistent with the Project WQMP. The WQMP shall follow the latest edition of the City of Huntington Beach Project Water Quality Management Plan Preparation Guidance Manual and shall be submitted with the first submittal of the project's Precise Grading Plan.
 9. A suitable location, as approved by the City, shall be depicted on the grading plan for the necessary trash enclosure(s). The area shall be paved with an impervious surface, designed not to allow run-on from adjoining areas, designed to divert drainage from adjoining roofs and pavements diverted around the area, and screened or walled to prevent off-site transport of trash. The trash enclosure area shall be covered or roofed with a solid, impervious material. Connection of trash area drains into the storm drain system is prohibited. If feasible, the trash enclosure area shall be connected into the sanitary sewer. (DAMP)
 10. A soils report, prepared by a Licensed Engineer, shall be submitted for reference only. (MC 17.05.150)
 11. The applicant's grading/erosion control plan shall abide by the provisions of AQMD's Rule 403 as related to fugitive dust control. (AQMD Rule 403)
 12. The name and phone number of an on-site field supervisor hired by the developer shall be submitted to the Planning and Public Works Departments. In addition, clearly visible signs shall be posted on the perimeter of the site every 250 feet indicating who shall be contacted for information regarding this development and any construction/grading-related concerns. This contact person shall be available immediately to address any concerns or issues raised by adjacent property owners during the construction activity. He/She will be responsible for ensuring compliance with the conditions herein, specifically, grading activities, truck routes, construction hours, noise, etc. Signs shall include the applicant's contact number, regarding grading and construction activities, and "1-800-CUTSMOG" in the event there are concerns regarding fugitive dust and compliance with AQMD Rule No. 403.
 13. The applicant shall notify all property owners and tenants within 300 feet of the perimeter of the property of a tentative grading schedule at least 30 days prior to such grading.

THE FOLLOWING DEVELOPMENT REQUIREMENTS SHALL BE COMPLIED WITH DURING GRADING OPERATIONS:

14. An Encroachment Permit is required for all work within the City's right-of-way. (MC 12.38.010/MC 14.36.030)
15. The developer shall coordinate the development of a truck haul route with the Department of Public Works if the import or export of material in excess of 5000 cubic yards is required. This plan shall include the approximate number of truck trips and the proposed truck haul routes. It shall specify the hours in which transport activities can occur and methods to mitigate construction-related impacts to adjacent residents. These plans must be submitted for approval to the Department of Public Works. (MC 17.05.210)
16. Water trucks will be utilized on the site and shall be available to be used throughout the day during site grading to keep the soil damp enough to prevent dust being raised by the operations. (California Stormwater BMP Handbook, Construction Wind Erosion WE-1)
17. All haul trucks shall arrive at the site no earlier than 8:00 a.m. or leave the site no later than 5:00 p.m., and shall be limited to Monday through Friday only. (MC 17.05)
18. Wet down the areas that are to be graded or that is being graded, in the late morning and after work is completed for the day. (WE-1/MC 17.05)
19. The construction disturbance area shall be kept as small as possible. (California Stormwater BMP Handbook, Construction Erosion Control EC-1) (DAMP)
20. All haul trucks shall be covered or have water applied to the exposed surface prior to leaving the site to prevent dust from impacting the surrounding areas. (DAMP)
21. Prior to leaving the site, all haul trucks shall be washed off on-site on a gravel surface to prevent dirt and dust from leaving the site and impacting public streets. (DAMP)
22. Comply with appropriate sections of AQMD Rule 403, particularly to minimize fugitive dust and noise to surrounding areas. (AQMD Rule 403)
23. Wind barriers shall be installed along the perimeter of the site. (DAMP)
24. All construction materials, wastes, grading or demolition debris and stockpiles of soils, aggregates, soil amendments, etc. shall be properly covered, stored and secured to prevent transport into surface or ground waters by wind, rain, tracking, tidal erosion or dispersion. (DAMP)

THE FOLLOWING DEVELOPMENT REQUIREMENTS SHALL BE COMPLETED PRIOR TO ISSUANCE OF A BUILDING PERMIT:

25. A Precise Grading Permit shall be issued. (MC 17.05)
26. Traffic impact fees for commercial development shall be paid at the rate applicable at the time of Building Permit issuance. The current rate of \$172 per net new added daily trip is adjusted annually. This project is forecast to generate 416 new daily trips for a total traffic impact fee of \$71,552.00. The rate is subject to an annual adjustment on December 1st. (MC 17.65)
27. A fair share fee shall be paid for the improvements identified in the Beach/Edinger Corridor Specific Plan at the intersection of Beach Boulevard at Edinger Avenue (the addition of a third westbound through lane and a fourth northbound through lane) and at the intersection of Beach Boulevard at McFadden Avenue (the addition of a separate northbound right turn lane and a separate southbound right turn lane). (BECSP)

**THE FOLLOWING DEVELOPMENT REQUIREMENTS SHALL BE COMPLETED PRIOR TO
ISSUANCE OF AN ENCROACHMENT PERMIT:**

28. Traffic Control Plans, prepared by a Licensed Civil or Traffic Engineer, shall be prepared in accordance with the latest edition of the City of Huntington Beach Construction Traffic Control Plan Preparation Guidelines and submitted for review and approval by the Public Works Department. (Construction Traffic Control Plan Preparation Guidelines)

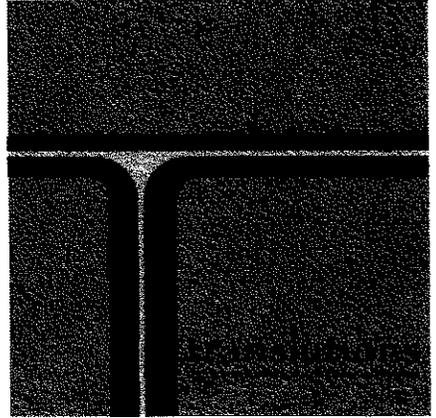
**THE FOLLOWING DEVELOPMENT REQUIREMENTS SHALL BE COMPLETED PRIOR TO FINAL
INSPECTION OR OCCUPANCY:**

29. Complete all improvements as shown on the approved grading plans. (MC 17.05)
30. All new utilities shall be undergrounded. (MC 17.64)
31. All applicable Public Works fees shall be paid at the current rate unless otherwise stated, per the Public Works Fee Schedule adopted by the City Council and available on the city web site at http://www.surfcity-hb.org/files/users/public_works/fee_schedule.pdf. (ZSO 240.06/ZSO 250.16)
32. Prior to grading or building permit close-out and/or the issuance of a certificate of use or a certificate of occupancy, the applicant shall:
 - a. Demonstrate that all structural Best Management Practices (BMPs) described in the Project WQMP have been constructed and installed in conformance with approved plans and specifications.
 - b. Demonstrate all drainage courses, pipes, gutters, basins, etc. are clean and properly constructed.
 - c. Demonstrate that applicant is prepared to implement all non-structural BMPs described in the Project WQMP.
 - d. Demonstrate that an adequate number of copies of the approved Project WQMP are available for the future occupiers.

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SEP 27 2012

Dept. of Planning
& Building



February 16, 2012

Ms. Cristal Garcia
Project Manager
SITE Design Group, Inc.
2777 Loker Avenue West, Suite C-2
Carlsbad, CA 92010

Re: Center Avenue Skate Park Parking Demand Analysis

Dear Ms. Garcia:

Translutions Inc. (Translutions) has prepared this analysis to determine the potential parking demand of the Center Avenue Skate Park and evaluate the adequacy of the proposed parking supply. The project is located on Center Avenue, approximately 500 feet east of Gothard Street in the City of Huntington Beach (City). The site is bounded by McFadden Avenue on the north, a Union Pacific Railroad track to the east, Center Avenue to the south, and Southern California Edison transmission lines to the west. The project would construct an approximately 2.7 acre skate park with 26 parking spaces, including 2 handicapped spaces. The project also proposes to set-aside a 20-foot-wide by 300-foot-long area for future installation of a transit platform.

Project Description

The project applicant proposes to construct a public skate park which would be comprised of the following elements:

- Approximately 14,000 square feet of skate park plaza area
- 13,000 square feet of skate bowl area
- A 3,500 square foot skate shop/concession/restroom building
- 15,000 square feet of turf/walking area
- A 480 square foot skate park restroom structure
- A 200 square foot entrance kiosk
- A main parking area with 24 regular and 2 handicapped parking spaces
- An overflow parking area that could accommodate approximately 40 passenger vehicles (to be used only during special events)

The skate park would operate seven days a week, from 10:00 a.m. to 10:00 p.m. It is anticipated that approximately 75 visitors would visit the skate park on a typical day. During peak days, a maximum of 130 visitors per day are expected. The primary purpose of patrons visiting the site would be to utilize the 14,000 square foot skate park plaza area and the 13,000 square foot skate bowl area. The skate shop/concession/restroom building is not expected to attract patrons by itself. Rather, this use would be a complementary amenity for patrons utilizing the skate park plaza and skate bowl areas. As such, the skate shop/concession/restroom building is not expected to generate additional parking demand.

Patrons of the skate park are expected to be youth who would typically utilize non-vehicular transportation such as transit, bikes and skateboards or be dropped off by others. Some patrons are expected to arrive by bus, as the project is located adjacent to the Golden West Transit Center. The Golden West Transit center is served by OCTA bus lines 29, 66, 70, 211 and 701. Additionally, the project proposes to set-aside a 20-foot-wide by 300-foot-long area for future installation of a transit platform, which will further facilitate non-vehicular travel to the site.

Special events may be hosted at the skate park up to 15 event days per year. Twelve of these event days would be held on weekends and would have an expected attendance of 300 to 500 spectators per day. The remaining three event days would draw up to 2,500 spectators per day, and would be held Friday through Sunday. Similar to regular operation of the skate park, a significant number of guests for these events are expected to be local youth, who would either be dropped off, or utilize non-vehicular forms of transportation. Parking for guests arriving by vehicle would be provided at the Huntington Beach Sports Complex, which has a total of 850 parking spaces. The Huntington Beach Sports Complex is located approximately 2.8 miles from the project site, and guests would be transported to the skate park via shuttle buses.

Methodology

The Center Avenue Skate Park is located within the Town Center Neighborhood segment of the Beach and Edinger Corridors Specific Plan (BECSP). The BECSP provides development standards for projects within the specific plan area, including minimum and maximum parking requirements. The BECSP does not specifically contain parking requirements for skate park land use, but the skate park is categorized as a civic and cultural use which does have the parking requirements. In order to deviate from the parking requirements of the civic and cultural use, a Variance is required under the BECSP. A parking demand study is one way to substantiate the parking deviation request. The Huntington Beach Zoning Code does not state a parking requirement for skate park land use, but does specify that parking for park and recreation facilities would be as specified in a Conditional Use Permit (CUP). Because neither the BECSP nor the Zoning Code provides a parking requirement for skate park land use, the parking demand was determined through observations conducted at similar skate park facilities.

As noted in the project description section, special events may be held at the project up to 15 event days per year. The skate park operator has developed a plan to provide off-site parking at the Huntington Beach Sports Complex, with shuttles to transport guests to and from the skate park. Because off-site parking for special events has already been considered in the project description, this parking analysis does not evaluate parking during special events. The parking demand analyzed below considers the parking demand that would be experienced during a typical day of skate park operation.

Translutions staff conducted parking utilization surveys at two similar skate parks on Saturday, February 11, 2012. Prior to conducting the surveys, Translutions staff contacted the parks and recreation department at the City of San Diego and Etnies Skate Park and discussed usage and peak patronage at the two parks. Based on the discussions, it was determined that the peak usage occurs between 12:00 noon and 2:00 p.m. on Saturdays. Therefore, Translutions staff conducted the surveys between 12:00 noon and 2:00 p.m. but observed the parks for a half hour before and after the peak usage period. During the survey it was observed that park usage and parking demand drops significantly outside of the peak 12:00 noon to 2:00 p.m. period. Therefore, this analysis reports and is based on the peak two hour period from noon to 2:00 p.m. The sites surveyed were the Etnies Skate Park in the City of Lake Forest, and the Carmel Valley Skate Park in the City of San Diego. The number of vehicles parked between noon and 2:00 p.m., as well as the number of participants present during these times were noted during the surveys.

Analysis

The parking utilization surveys are summarized in Table A and Table B. Table A shows the survey data for Etnies Skate Park and Table B shows the survey data for Carmel Valley Skate Park. As shown in Table A the on-site parking lot for Etnies Skate Park provides 12 marked spaces (11 regular + 1 ADA). The survey shows that parking is generally sufficient, although some vehicles were observed along the red curb in front. During peak operations, vehicles were observed dropping skaters off and leaving the parking lot.

Table A: Parking Utilization Survey (Lake Forest)

| Etnies Skate Park (38,000 Square Feet) - City of Lake Forest | | | | | |
|--|---------|-----|----------------|-------|---------|
| Total Parking Supply = 11 Regular + 1 ADA | | | | | |
| Parking Demand | | | | | Skaters |
| Time | Regular | ADA | Along Red Curb | Total | |
| 12:00 PM | 7 | 0 | 3 | 10 | 28 |
| 12:15 PM | 9 | 0 | 3 | 12 | 28 |
| 12:30 PM | 10 | 0 | 2 | 12 | 32 |
| 12:45 PM | 10 | 1 | 2 | 13 | 30 |
| 1:00 PM | 11 | 1 | 2 | 14 | 31 |
| 1:15 PM | 11 | 1 | 1 | 13 | 36 |
| 1:30 PM | 10 | 1 | 2 | 13 | 42 |
| 1:45 PM | 7 | 1 | 3 | 11 | 42 |
| 2:00 PM | 5 | 1 | 3 | 9 | 39 |
| Maximum Parking Demand | | | | 14 | 42 |

As shown in Table B the on-site parking lot for Carmel Valley Skate Park provides 18 marked spaces (16 regular + 2 ADA). The survey shows that this park provides sufficient parking spaces on site for the existing use.

Table B: Parking Utilization Survey (San Diego)

| Carmel Valley Skate Park (13,500 Square Feet) - City of San Diego | | | | | |
|---|---------|-----|----------------|-------|---------|
| Total Parking Supply = 16 Regular + 2 ADA | | | | | |
| Parking Demand | | | | | Skaters |
| Time | Regular | ADA | Along Red Curb | Total | |
| 12:00 PM | 3 | 0 | 0 | 3 | 14 |
| 12:15 PM | 3 | 0 | 0 | 3 | 15 |
| 12:30 PM | 9 | 1 | 0 | 10 | 23 |
| 12:45 PM | 5 | 0 | 0 | 5 | 17 |
| 1:00 PM | 7 | 0 | 0 | 7 | 22 |
| 1:15 PM | 12 | 0 | 0 | 12 | 30 |
| 1:30 PM | 13 | 0 | 0 | 13 | 34 |
| 1:45 PM | 9 | 0 | 0 | 9 | 21 |
| 2:00 PM | 12 | 0 | 0 | 12 | 22 |
| Maximum Parking Demand | | | | 13 | 34 |

The analysis shows that there is no linear relationship between maximum parking demand and the area of a skate park. Although Etnies Skate Park is approximately three times the size of Carmel Valley Skate Park, the maximum parking demand observed during peak patronage hours is almost the same. The maximum demand observed in both sites was similar (14 for Etnies Skate Park and 13 for Carmel Valley Skate Park).

Further, it was observed that there is no direct correlation between maximum number of skaters and parking demand. As shown in Tables A the peak parking demand at Etnies Skate Park did not occur at the same time the peak number of skaters was observed. This observation indicates that many of the skaters are dropped-off, or use non-vehicular transportation. It was also observed that the number of skaters that can utilize the bowl or plaza runs at one time is limited. In each park, most of the observed skaters were waiting to skate, and at any given time only a few skaters were in the bowl or plaza runs. The capacity of the park is therefore limited by the number of skaters that can utilize the bowl or plaza runs, and the amount of time other skaters are willing to wait. Therefore, the square footage of the park is also not a good indicator of parking demand.

Conclusion

The proposed Center Avenue Skate Park is a 27,000 square foot facility and falls between the size of the two surveyed skate parks (38,000 and 13,500). Based on the survey data, Translutions concludes that there is no direct correlation between the number of skaters, the area of the site, and the maximum parking demand. Further, it was observed that many skaters are dropped off or use non-vehicular transportation. It should be noted that the proposed Center Avenue Skate Park is adjacent to the Golden West Transit Center, which is likely to encourage increased transit use by patrons.

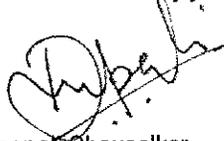
In the absence of a parking rate per square foot or skater, Translutions recommends that the project provide parking to meet the maximum demand observed at Etnies Skate Park, the larger of the two surveyed parks. The maximum parking demand at Etnies Skate Park was 14 spaces.

The project will provide 26 parking spaces (24 regular and 2 handicapped parking spaces). Based on the surveys conducted at Etnies Skate Park in Lake Forest and Carmel Valley Skate Park in San Diego, the proposed 26 parking spaces would be adequate to accommodate the parking demand of the project.

Translutions trusts you will find this information helpful for your planning purposes. We appreciate the opportunity to provide this analysis. If you have any questions, please do not hesitate to contact me at (949) 336-7330.

Sincerely,

Translutions, Inc.



Deepali Chausalkar
Principal

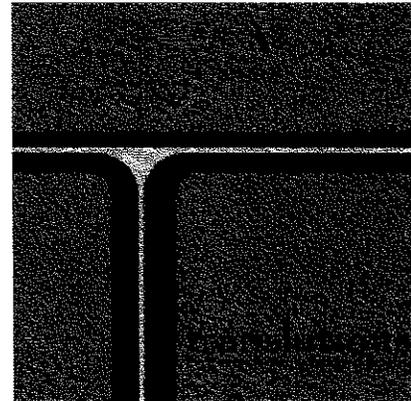


Prepared under the supervision of Joseph Jimenez, T.E.

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SEP 27 2012

Dept. of Planning
& Building



September 25, 2012

Ms. Cristal Garcia
Project Manager
SITE Design Group, Inc.
2777 Loker Avenue West, Suite C-2
Carlsbad, CA 92010

Re: Vans Skate Park Huntington Beach – Special Events Parking Demand Analysis

Dear Ms. Garcia:

Translutions Inc. (Translutions) has prepared this analysis to determine the potential parking demand during special events at the proposed Vans Skate Park to be located in the City of Huntington Beach. Translutions had previously prepared a parking demand analysis for the project for regular operations. The project is located on Center Avenue, approximately 500 feet east of Gothard Street in the City of Huntington Beach (City). The site is bounded by McFadden Avenue on the north, a Union Pacific Railroad track to the east, Center Avenue to the south, and Southern California Edison transmission lines to the west.

Special events may be hosted at the skate park up to 15 event days per year. Twelve of these event days would be held on weekends and would have an expected attendance of 300 to 500 spectators per day. The remaining three event days would draw up to 2,500 spectators per day, and would be held Friday through Sunday. Similar to regular operation of the skate park, a significant number of guests for these events are expected to be local youth, who would either be dropped off, or utilize non-vehicular forms of transportation. Parking for guests arriving by vehicle would be provided at the Huntington Beach Sports Complex, which has a total of 850 parking spaces. The Huntington Beach Sports Complex is located approximately 2.8 miles from the project site, and guests would be transported to the skate park via shuttle buses.

This analysis has been conducted based on discussions with City of Lake Forest staff at the Etnies Skate Park facility. Since the next planned special event at Etnies Skate Park is in November, a field survey could not be conducted. Therefore, this analysis is based on discussions with Etnies Skate Park staff. Based on discussion with Etnies Skate Park staff, during the special events held at the park, about 2,500 attendees visit the park during the course of the day. However, at any given time, there are approximately 400 to 450 attendees in the facility. Etnies Skate Park staff mentioned that the maximum amount of parking they have utilized for any event is between 80 to 90 spaces. Most attendees park at the Business Park across the Serrano Creek Trail, south-east to the skate park. The Business Park has approximately 500 parking spaces, but the maximum use during special events at the skate park is 80 to 90 spaces. To present a conservative analysis, this analysis assumes a rate of 100 parking spaces per 400 attendees (or a rate of 0.25 per attendee).

As discussed earlier, based on attendance data from Etnies Skate Park, which has events with 2,500 daily attendees (the proposed project is anticipated to have the same during large special events), the peak number of attendees is no more than 450 at any given time. However, this analysis assumes that all 500 attendees during small events and 2,500 attendees during large events will be on site at one time.

To validate the accuracy of the rates, Translutions researched rates for other large congregation areas from the Institute of Transportation Engineers' (ITE) *Parking Generation Manual*. This data is presented for informational purposes only to show a comparative analysis of the rates used in this analysis to other land uses of mass congregation. Table A shows parking rates for various land uses.

| Land Use | Rate |
|--|-------------|
| Movie Theater (per seat, Friday) | 0.26 |
| Multiplex Movie Theater (per seat, Friday) | 0.15 |
| Church (per seat, Sunday) | 0.20 |
| Rate used for Skate Park Special Events | 0.25 |

As seen in Table A, except for a Movie Theater which parks at 0.26 spaces per seat, the other land uses of mass congregations have a lower parking demand than that used for the current analysis.

Table B Shows the parking demand and supply analysis for the project. As shown in Table B, the parking demand for the project during small events of up to 500 attendees is anticipated to be 125 spaces and during large events is anticipated to be 625 spaces. Since there are 850 available spaces at the Huntington Beach Sports Complex, it follows that a surplus of 725 spaces during small events and 125 spaces during large events will remain after usage by the project.

| | |
|---|------------|
| Number of Attendees | 400 |
| Number of Parking Spaces | 100 |
| Parking Rate (per attendee) | 0.25 |
| Forecast Parking Demand for Small Events (500 attendees) | 125 |
| Forecast Parking Demand for Large Events (2,500 attendees) | 625 |
| Available Spaces at Sports Complex | 850 |
| Parking Surplus During Small Events | 725 |
| Parking Surplus During Large Events | 125 |

Other Considerations

In addition, it should be noted that the Etnies Skate Park is not well served by transit, whereas this project is located adjacent to the Golden West Transit Center. The Golden West Transit center is served by OCTA bus lines 29, 66, 70, 211 and 701. Additionally, the project proposes to set-aside a 20-foot-wide by 300-foot-long area for future installation of a transit platform, which will further facilitate non-vehicular travel to the site. Therefore, the parking demand for the project is likely to be less than the number anticipated. Therefore, although no reduction has been taken for higher transit accessibility, parking demand at the proposed project is likely to be less than at Etnies Skate Park since it is better served by transit.

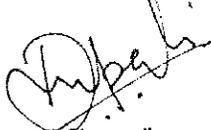
Conclusion

Based on the above analysis, it is anticipated that the parking available at the Huntington Beach Sports Complex will be sufficient to meet the demands of the project during special events. A surplus of 725 parking spaces will be available during small events and 125 parking spaces will be available during large events for other uses at the Sports Complex. It is recommended that the project operators coordinate with the Sports Complex prior to organizing special events so that both facilities do not organize special events on the same day.

Translutions trusts you will find this information helpful for your planning purposes. We appreciate the opportunity to provide this analysis. If you have any questions, please do not hesitate to contact me at (949) 336-7330.

Sincerely,

Translutions, Inc.



Deepati Chausalkar
Principal



Prepared under the supervision of Joseph Jimenez, T.E.

CENTER AVENUE SKATE PARK PROJECT

**Findings of Fact/
Statement of Overriding Considerations**

Prepared for

City of Huntington Beach
Planning and Building Department
2000 Main Street, Third Floor
Huntington Beach, California 92648

Prepared by

PCR Services Corporation
1 Venture Plaza, Suite 150
Irvine, CA 92618

March 13, 2012

ATTACHMENT NO. 7.1

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ATTACHMENT NO. 7.2

CHAPTER 1 INTRODUCTION

This document presents the Findings of Fact and Statement of Overriding Considerations that must be adopted by the City of Huntington Beach (City) pursuant to the requirements of Sections 15091 and 15093, respectively, of the California Environmental Quality Act Guidelines (CEQA Guidelines) prior to the approval of the Center Avenue Skate Park Project (proposed project).

This document is organized as follows:

- Chapter 1** Introduction to the Findings of Fact and Statement of Overriding Considerations.
- Chapter 2** Presents the CEQA Findings of the Environmental Impact Report (EIR), including the identified significant land use and noise impacts.
- Chapter 3** Presents the alternatives to the proposed project and evaluates them in relation to the findings contained in Section 15091(a)(3) of the CEQA Guidelines. The City must consider and make findings regarding alternatives when a project would involve environmental impacts that cannot be reduced to a less-than-significant level, or cannot be substantially reduced, by proposed mitigation measures.
- Chapter 4** Presents a Statement of Overriding Considerations that is required in accordance with Section 15093 of the CEQA Guidelines for significant impacts of the proposed project that cannot be mitigated to a less-than-significant level.

The project applicant proposes to lease vacant property from the City to design, develop, maintain and operate a public skate park. The proposed project includes approximately 14,000 square feet of skate park plaza area, 13,000 square feet of skate bowl area, a 3,500-square-foot skate shop/concession/restroom building, 15,000 square feet of turf/walking area, a 480-square-foot skate park restroom structure, a 200-square-foot skate park entrance kiosk, the main parking lot near the primary site access fronting Center Ave, and a secondary parking area off McFadden Ave to be used only for special events. The project would include extensive landscaping and turf areas, sidewalks, walkways, trash/recycling facilities, drinking fountains, and restrooms, all of which would be accessible to the public. Additionally, in order to allow for potential future development of a transit stop, the proposed project includes the dedication of a "Transit Reserve Area."

The project as proposed would result in significant unavoidable project-level impacts with respect to land use and noise. In comparison to the alternatives analyzed against the proposed development, the City finds in the Draft EIR that Alternative 3, the Alternate Location Alternative, would be considered the environmentally superior alternative.

The following discretionary approvals by the City of Huntington Beach are required to implement the proposed project:

- Certification of Environmental Impact Report (Planning Commission)

ATTACHMENT NO. 7.3

- General Plan Amendment (Planning Commission and City Council)
- Zoning Text Amendment (Planning Commission and City Council)
- Site Plan Review (Planning Commission)
- Variance (Planning Commission)

ATTACHMENT NO. 7.4

CHAPTER 2 CEQA FINDINGS

2.1 INTRODUCTION

This chapter presents the potential impacts that were identified in the EIR and the findings that are required in accordance with Section 15091 of the CEQA Guidelines. The possible findings for each significant and/or potentially significant adverse impact are as follows:

- 1) Changes or alterations have been required in, or incorporated into the project which avoid, substantially lessen, or reduce the magnitude of the significant environmental effect as identified in the EIR ("Finding 1").
- 2) Such changes or alterations are within the responsibility and jurisdiction of another public agency and not the agency making the findings. Such changes have been adopted by such other agency or can, and should be, adopted by such other agency ("Finding 2").
- 3) Specific economic, social, or other considerations make infeasible the mitigation measures or project alternatives in the EIR ("Finding 3").

CEQA requires that the lead agency adopt mitigation measures or alternatives, where feasible, to avoid or substantially reduce significant environmental impacts that would otherwise occur as a result of a project. Project modification or alternatives are not required, however, where they are infeasible or where the responsibility for modifying the project lies with some other agency (State CEQA Guidelines §15091, subdivision (a), [3]). Public Resources Code Section 21061.1 defines "feasible" to mean "capable of being accomplished in a successful manner within a reasonable period of time, taking into account economic, environmental, social and technological factors." (See also *Citizens of Goleta Valley v. Board of Supervisors* [Goleta II] [1990] 52 Cal.3d 553, 565 [276 Cal. Rptr. 410].)

Only after fully complying with the findings requirement can an agency adopt a Statement of Overriding Considerations (*Citizens for Quality Growth v. City of Mount Shasta* [1988] 198 Cal.App.3d 433, 442, 445 [243 Cal. Rptr. 727]). CEQA requires the Lead Agency to state in writing the specific rationale to support its actions based on the Final EIR and/or information in the record. This written statement is known as the Statement of Overriding Considerations. The Statement of Overriding Considerations provides the information that demonstrates the decision-making body of the Lead Agency has weighed the benefits of the project against its unavoidable adverse effects in determining whether to approve the project. If the benefits of the project outweigh the unavoidable adverse environmental effects, the adverse effects may be considered "acceptable."

The California Supreme Court has stated that, "the wisdom of approving any development project, a delicate task which requires a balancing of interests, is necessarily left to the sound discretion of the local officials and their constituents who are responsible for such decisions. The law as we interpret and apply it simply requires that those decisions be informed, and therefore balanced." (Goleta II, 52 Cal.3d 553, 576 [276 Cal. Rptr. 401].)

ATTACHMENT NO. 7.5

This document presents the City of Huntington Beach findings as required by CEQA, cites substantial evidence in the record in support of each of the findings, and presents an explanation to supply the logical step between the finding and the facts in the record (State CEQA Guidelines §15091). Additional facts that support the findings are set forth in the Draft EIR, the Final EIR, staff reports, and the record of proceedings.

Table 2-1, *CEQA Findings for the Center Avenue Skate Park Project*, below, summarizes the potentially significant impacts of the proposed project identified in the Draft EIR that were reduced to less-than-significant levels with mitigation, as well as the project-level significant unavoidable impacts.

ATTACHMENT NO. 7.6

Table 2-1

CEQA Findings for the Center Avenue Skate Park

| Impact Statement | Impact Summary | Findings |
|---|---|--|
| <p>E. LAND USE AND PLANNING</p> <p>Impact 4.E-1 The project would result in less than significant land use impacts with regard to consistency with the SCAG Regional Transportation Plan (RTP), SCAG Regional Comprehensive Plan (RCP), the Beach and Edinger Corridors Specific Plan (BECSP) and the Huntington Beach Zoning and Subdivision Ordinance. However, land use impacts with respect to conflicts with the SCAG Regional Housing Needs Assessment (RHNA) and City of Huntington Beach General Plan would be significant and unavoidable.</p> | <p>The proposed project would not result in conflicts with the SCAG RTP, SCAG RCP, BECSP, or HBZSO; however, the loss of potential on-site affordable housing units as identified in the SCAG RHNA and the City's General Plan Housing Element would conflict with the housing allocation requirements of these plans, which is considered a significant unavoidable impact, since the timing of rezoning an alternative site within the City that could provide such housing is uncertain at this time. Further, special event noise at the skate park would exceed established thresholds and therefore would conflict with applicable goals and policies in the City's General Plan related to noise generation.</p> | <p>Finding 3. The proposed project would reduce the availability of sites within the City of Huntington Beach to provide affordable housing as required in the SCAG RHNA and the City's General Plan Housing Element, and would also conflict with applicable goals and policies of the City's General Plan regarding the generation of noise. As such, the proposed project would have a significant unavoidable land use impact, and no feasible mitigation is available. The City finds specific economic, social, or other considerations make infeasible the project alternatives in the EIR.</p> |
| <p>F. NOISE</p> <p>Impact 4.F-3 Project implementation would have a minimal effect on the existing noise environment within and adjacent to the Project Area during normal skate park and retail operations. Thus, long-term noise impacts under normal operation would be less than significant. However, periodic special events would temporarily exceed the allowable noise thresholds at adjacent noise-sensitive residential uses. Thus, operational noise impacts are considered significant and unavoidable.</p> | <p>Project-related noise generation would remain below established thresholds during normal day-to-day operation of the proposed skate park and retail use. However, crowd noise and noise from amplified music and announcements during periodic special events would exceed the City's noise thresholds and impacts would be significant and unavoidable even with implementation of applicable mitigation measures.</p> | <p>Finding 3. The proposed project would generate noise levels during special events that would exceed the City's established noise thresholds. As such, the proposed project would have a significant unavoidable noise impact, and no feasible mitigation is available. The City finds specific economic, social, or other considerations make infeasible the project alternatives in the EIR.</p> |

Table 2-1
CEQA Findings for the Center Avenue Skate Park

| Impact Statement | Impact Summary | Findings |
|--|---|---|
| <p>G. TRAFFIC/TRANSPORTATION Impact 4.G-3 The project would provide on-site parking to accommodate the day-to-day needs of guests and employees to the site. Special events could result in off-site parking impacts to neighboring uses. However, implementation of mitigation measure 4.G-1 would ensure that potentially significant parking impacts to neighboring uses during special events are reduced to a less than significant level.</p> | <p>Parking for the skate park and retail use would be provided on-site and is considered adequate to meet projected parking demands of the proposed uses. However, during special events at the skate park, an off-site parking program (per mitigation measure 4.G-1) would be required to be implemented to reduce potentially significant short-term parking impacts to less than significant.</p> | <p>Finding 1. The City finds that the identified changes or alterations in the project, which would reduce Impact 4.G-3 to less-than-significant levels, are hereby incorporated into the project. No additional mitigation measures are necessary with the implementation of mitigation measure 4.G-1.</p> |

CHAPTER 3 FINDINGS REGARDING PROJECT ALTERNATIVES

3.1 INTRODUCTION

The EIR prepared for the Center Avenue Skate Park Project considered three separate alternatives to the proposed project. Pursuant to Section 15126.6(a) of the CEQA Guidelines, the primary intent of an alternatives evaluation is to “describe a range of reasonable alternatives to the project, or to the location of the project, which would feasibly attain most of the basic objectives of the project but would avoid or substantially lessen any of the significant effects of the project, and evaluate the comparative merits of the alternatives.”

This chapter describes the project objectives used to evaluate project alternatives presented in the Draft EIR and recommend the proposed project. A description of the alternatives compared to the proposed project and the findings regarding the feasibility of adopting the described alternatives is presented for use by the City in the decision-making process.

3.2 PROJECT OBJECTIVES

The objectives of the proposed project identified by the City and project applicant, respectively, are presented below.

City Objectives

- Implement the policies and development standards of the City’s General Plan, Beach and Edinger Corridors Specific Plan (BECSP), and the Zoning and Subdivision Ordinance (ZSO) as referred to in the BECSP.
- Create a development that is compatible with and sensitive to the existing land uses in the project area.
- Enhance the community image of Huntington Beach through the design and construction of a high quality master skate park that attracts users from across the City.
- Minimize development and operational cost to the City by partnering with a private equity partner.
- Mitigate environmental impacts to the greatest extent possible.

Applicant Objectives

- Develop a skate park facility that is free of admission and open to the public.
- Build a new master skate park facility large enough to meet the current and future demand of Huntington Beach skate board enthusiasts.
- Locate a master skate park in an area with nearby public amenities that support skate park users, such as public transit, accessible pathways, trees and benches, and restrooms within a reasonable distance.
- Provide a state-of-the-art skate park facility designed to allow for innovative programming to meet the needs of a culturally diverse and multi-generational skate board enthusiast population.

- Develop a skate park in a location that is readily accessible, highly visible, and provides a safe environment for visitors.

3.3 SELECTION OF ALTERNATIVES

The range of feasible alternatives was selected and discussed in a manner to foster meaningful public participation and informed decision-making. Among the factors that were taken into account when considering the feasibility of alternatives (as described in CEQA Guidelines Section 15126.6[f][1]) were environmental impacts, economic viability, availability of infrastructure, regulatory limitations, jurisdictional boundaries, and attainment of project objectives. As stated in Section 15126.6(a) of the CEQA Guidelines, an EIR need not consider an alternative whose effects could not be reasonably identified, whose implementation is remote or speculative, or one that would not achieve the basic project objectives. The analysis includes sufficient information about each alternative to provide meaningful evaluation, analysis and comparison with the proposed project.

3.4 ALTERNATIVE FINDINGS

The following is a description of the alternatives evaluated in comparison to the proposed project, as well as a description of the specific economic, social, or other considerations that make them infeasible for avoiding or lessening the impacts. The City finds that the adoption of any of the alternatives to the project is infeasible. The reasons for each finding are provided following the description of the alternative, and are further described in the Draft EIR.

As described in Chapter 5, *Alternatives*, of the Draft EIR, three alternatives were evaluated in comparison to the proposed project. The environmental advantages and disadvantages of each of these alternatives are described. The alternatives that were selected for analysis include:

- **Alternative 1 – No Project/No Development Alternative** - Under the No Project/No Development Alternative, no improvements to the project site would occur, and the site would remain in its vacant, undeveloped state. Additionally, the project site would remain designated as “Residential Required” under the BECSP, with the potential to accommodate up to 175 affordable housing units.
- **Alternative 2 – Reduced Project Alternative** - Under the Reduced Project Alternative, the proposed skate park and retail/concession use would be constructed and operated at the project site, but would be reduced in terms of skate facilities and retail/concession intensity. Specifically, the proposed skate park and retail/concession use would be constructed at the project site, but would exclude the skate bowl area, reduce the skate plaza area to 8,000 square feet, and reduce the retail/concession use to 2,000 square feet. This represents a reduction of 13,000 square feet of skate bowl area, 6,000 square feet of skate plaza area, and 1,500 square feet of retail/concession floor area relative to the proposed project. Despite the reduction in development intensity on-site under this Alternative, it is assumed that special events would still be held at the reduced skate park
- **Alternative 3 – Alternate Location Alternative** - Under the Alternate Location Alternative the proposed project would be developed with the same skate park and retail/concession facilities and development intensity as the proposed project, but at another location in the City. Specifically, the proposed skate park and retail/concession use would be constructed and operated on County-owned property at the former Gothard Landfill site, which is located at 18131 Gothard Street, on west side of Gothard Street south of Talbert Avenue. The project would be built on a 3.5-acre portion of the

approximately 11.5-acre property along the eastern side of the site fronting Gothard Street, and would include all improvements contemplated as part of the proposed project, including on-site parking and additional space for special event parking and turf/vendor areas. Specifically, the northern half of the site would be developed with an above-grade skate bowl area, a 15,000 square-foot above-grade skate plaza area, a 4,000-square-foot retail/concession use and restroom structure, a turf area surrounding the skate plaza (which would also contain temporary spectator seating during special events), and on-site surface parking lot with 40 parking spaces. The southern half of the alternate site would remain undeveloped to provide space for special event parking. Primary vehicle access would be provided by a driveway at the northeastern corner of the site on Gothard Street, while a secondary access would be located at the southeast corner of the property and would only be used during special events. As the site is currently designated for industrial uses in the City's General Plan and zoned for open space/recreation, amendments to the City's General Plan and Zoning Code would be required under this Alternative. Additionally, since the site is a former landfill, various structures housing equipment to capture landfill gases are located throughout the site and would remain on-site under this Alternative to address landfill gas-related hazards. This site may also contain sensitive biological resources, such as coastal sage scrub habitat, though the extent of such resources has not yet been determined.

Alternatives Considered but Eliminated from Further Evaluation

In accordance with CEQA Guidelines Section 15126.6(c), an EIR should identify any alternatives that were considered for analysis but rejected as infeasible and briefly explain the reasons for their rejection. According to the CEQA Guidelines, among the factors that may be used to eliminate alternatives from detailed consideration are the alternative's failure to meet most of the basic project objectives (outlined above), the alternative's infeasibility, or the alternative's inability to avoid significant environmental impacts. Given the relatively specific objectives of the proposed project and the limited scope of proposed uses, no additional project Alternatives were considered for analysis in this EIR.

No Project/No Development Alternative

Alternative 1, the No Project/No Development Alternative, would not result in any physical changes to the project site and no development would occur. The No Project/No Development Alternative would result in reduced impacts with regard to all environmental issues except for aesthetics (operational visual quality) and hydrology and water quality (operational water quality), which would be greater than under the proposed project, and would avoid significant unavoidable land use and noise impacts that would occur under the proposed project.

Alternative 1 would fail to meet any of the project's goals and objectives, either partially or fully.

Findings

The City hereby finds that the No Project/No Development Alternative is infeasible for the following environmental, economic, social, and other considerations:

- Would not enhance the community image of Huntington Beach through the design and construction of a high quality master skate park that attracts users from across the City.

- Would not minimize development and operational cost to the City by partnering with a private equity partner.
- Would not develop a skate park facility that is free of admission and open to the public.
- Would not build a new master skate park facility large enough to meet the current and future demand of Huntington Beach skate board enthusiasts.
- Would not locate a master skate park in an area with nearby public amenities that support skate park users, such as public transit, accessible pathways, trees and benches, and restrooms within a reasonable distance.
- Would not provide a state-of-the-art skate park facility designed to allow for innovative programming to meet the needs of a culturally diverse and multi-generational skate board enthusiast population.
- Would not develop a skate park in a location that is readily accessible, highly visible, and provides a safe environment for visitors.

Reduced Project Alternative

Under Alternative 2, the Reduced Project Alternative, the proposed skate park and retail/concession use would be constructed and operated at the project site, but would be reduced in terms of skate facilities and retail/concession intensity. Alternative 2 would result in less impacts regarding aesthetics (visual character and light and glare), air quality (localized and regional construction emissions and operational emissions, AQMP consistency, pollutant concentrations, and odors), greenhouse gas emissions (GHG emissions), hydrology and water quality (hydrology, drainage, and water quality), noise (construction and operational noise and vibration), and transportation/traffic (intersection LOS, CMP impacts, access/circulation, alternative transportation plan consistency). This Alternative would also result in similar impacts regarding greenhouse gas emissions (GHG plan consistency), land use (plan consistency), and transportation/traffic (parking). This Alternative would not result in any impacts greater than those under the proposed project.

Alternative 2 would at least partially meet many of the project's goals and objectives, but would not achieve all goals and objectives to the extent the project would.

Findings

The City hereby finds that the Reduced Project Alternative is infeasible for the following environmental, economic, social, and other considerations:

- Would not build a new master skate park facility large enough to meet the current and future demand of Huntington Beach skate board enthusiasts.
- Would not provide a state-of-the-art skate park facility designed to allow for innovative programming to meet the needs of a culturally diverse and multi-generational skate board enthusiast population.

Alternate Location Alternative

Under the Alternate Location Alternative the proposed project would be developed with the same skate park and retail/concession facilities and development intensity as the proposed project, but at another location in the City. Alternative 3 would result in similar impacts regarding aesthetics (construction and operational

visual character and light/glare), air quality (AQMP consistency, construction emissions, and operational emissions), greenhouse gas emissions (GHG emissions and GHG plan consistency), hydrology and water quality (hydrology, drainage, and water quality), noise (violation of noise standards, groundborne vibration, and permanent noise increases), and traffic/transportation (intersection LOS, access/circulation, and alternative transportation). Alternative 3 would result in less impacts regarding air quality (exposure to substantial pollutant concentrations and odors), land use (plan consistency), noise (temporary or periodic noise increases), and transportation/traffic (parking). Alternative 3 would not result in any impacts greater than those under the proposed project among those issues analyzed in the EIR. However, Alternative 3 would result in greater impacts than the proposed project with regard to biological resources, geology and soils, and hazards and hazardous materials.

Alternative 3 would fully meet all of the project objectives.

Findings

The City hereby finds that the Alternate Location Alternative is infeasible for the following environmental, economic, social, and other considerations:

- Would not be located on a site that is owned by the City of Huntington Beach and available for development, and therefore would not minimize development cost.
- Would result in potential additional environmental impacts related to biological resources (sensitive habitat) and hazardous materials (landfill gas).

CHAPTER 4 STATEMENT OF OVERRIDING CONSIDERATIONS

4.1 INTRODUCTION

Section 15093 of the CEQA guidelines states:

- (a) CEQA requires the decision-making agency to balance, as applicable, the economic, legal, social, technological, or other benefits of a proposed project against its unavoidable environmental risks when determining whether to approve the project. If the specific economic, legal, social, technological, or other benefits of a proposed project outweigh the unavoidable adverse environmental effects, the adverse environmental effects may be considered "acceptable."
- (b) When the lead agency approves a project which will result in the occurrence of significant effects which are identified in the final EIR but are not avoided or substantially lessened, the agency shall state in writing the specific reason to support its actions based on the final EIR and/or other information in the record. The statement of overriding considerations shall be supported by substantial evidence in the record.
- (c) If an agency makes a statement of overriding considerations, the statement should be included in the record of the project approval and should be mentioned in the notice of determination.

The City of Huntington Beach (City) proposes to adopt a Statement of Overriding Considerations regarding the significant project-specific land use and noise impacts of the proposed project. Although all other project-level impacts are reduced to less-than-significant levels, this section describes the anticipated economic, social, and other benefits or other considerations of the proposed project to support the decision to proceed with the project even though two identified project-specific impacts are not mitigated to a less-than-significant level.

4.2 SIGNIFICANT ADVERSE IMPACTS

The City is proposing to approve the proposed project, with revisions to reduce environmental impacts, and has prepared an EIR required by CEQA. Even with revisions in the project, the following impacts are unavoidable because it has been determined that no feasible mitigation is available. Refer to Chapter 2 (CEQA Findings) for further clarification regarding the impacts listed below.

Land Use and Planning

- **Project-Specific** – The proposed project would result in conflicts with the SCAG RHNA for 2006-2014 and the applicable goals and policies of the Huntington Beach General Plan.

Noise

- **Project-Specific** – The proposed project would result in temporary noise increases associated with periodic special events on-site, during which noise levels would exceed established thresholds at nearby sensitive receptors.

4.3 FINDINGS

The City has evaluated all feasible mitigation measures and project revisions with respect to the project's impacts (see Chapter 2, CEQA Findings). The City has also examined a reasonable range of alternatives to the proposed project (see Chapter 3, Findings Regarding Project Alternatives). Based on this examination, the City has determined that because of its location (i.e., not located near residential uses or other noise-sensitive uses, or on a site designated for affordable housing units) Alternative 3, the Alternate Location Alternative, is considered to be the environmentally superior alternative. All of the other alternatives listed above would potentially result in lesser environmental impacts than the proposed project, although not necessarily less than significant. However, the City finds all three of the evaluated alternatives infeasible and less desirable than the proposed project and has rejected these alternatives from further consideration because they would not achieve the environmental, economic, social, and other considerations outlined in Chapter 3 (Findings Regarding Project Alternatives).

4.4 OVERRIDING CONSIDERATIONS

Specific economic, social, or other considerations outweigh the project-specific land use and noise impacts stated above. The reasons for proceeding with the proposed project, even though identified project-specific impacts are not fully mitigated to a less-than-significant level, are described below.

Project Benefits

1. The proposed project would provide a new state-of-the-art skate park and associated retail/concession use that would be located in close proximity to public transit facilities and residential communities.
2. The project would provide additional year-round, fee-free recreational opportunities within the City through provision of a state-of-the-art skate park on land owned by the City.
3. The project would increase the City's tax revenue through sales tax from retail sales and periodic special events.
4. The project would improve the visual quality and character of the project site through implementation of the design standards contained in the BECSP, including architectural features, lighting, signage, and landscaping.
5. The project would minimize consumption of natural resources through implementation of sustainability features such as drought-tolerant landscaping, efficient lighting and plumbing fixtures, skylights, and waste recycling facilities.

MITIGATION MONITORING AND REPORTING PROGRAM

This Mitigation Monitoring and Reporting Program (MMRP) has been prepared for the Center Avenue Skate Park Project (the "proposed project") in compliance with Section 21081.6 of the Public Resources Code and Section 15097 of the CEQA Guidelines, which is required for all projects where an Environmental Impact Report (EIR) or Mitigated Negative Declaration has been prepared. Section 21081.6 of the Public Resources Code states: " ...the [lead] agency shall adopt a reporting or monitoring program for the changes made to the project or conditions of project approval, adopted in order to mitigate or avoid significant effects on the environment...[and the program] shall be designed to ensure compliance during project implementation." The City of Huntington Beach is the Lead Agency for the proposed project.

This MMRP identifies the mitigation measures prescribed in the Final EIR to reduce the proposed project's potentially significant environmental impacts to a less than significant level. The MMRP for the proposed project will be in place through all phases of project implementation. The City shall be responsible for administering the MMRP activities to its staff, other City departments (e.g., Public Works Department), consultants, and/or contractors. The City will also ensure that mitigation monitoring is documented through reports and that deficiencies are promptly corrected. The designated environmental monitor (e.g., City building inspector, project contractor, certified professionals, etc., depending on the provisions specified below) will track and document compliance with mitigation measures, note any problems that may result, and take appropriate action to remedy problems. The MMRP lists mitigation measures according to the same numbering system contained in the EIR sections. Each mitigation measure is categorized by topic, with an accompanying discussion of the following:

- The monitoring phase of the project during which the mitigation measure should be monitored (i.e., Operation, Construction, or Prior to Construction Activities);
- The monitoring frequency of the mitigation measures (i.e., during periodic field inspection); and
- The enforcement agency (i.e., the agency with the authority to enforce the mitigation measure).

The MMRP is included as **Table 1**, *Mitigation Monitoring and Reporting Program*, below.

Table 1
Mitigation Monitoring and Reporting Program

| Mitigation Measure | Implementation Documentation | Timing | Monitoring Activity | Responsible Monitor | Compliance Verification Signature | Date |
|--|--|--|---|---------------------|-----------------------------------|-------|
| Aesthetics | | | | | | |
| BECSP MM4.1-2 Proposed new structures shall be designed to maximize the use of non-reflective façade treatments, such as matte paint or glass coatings. Prior to issuance of building permits for the proposed project, the Applicant shall indicate provision of these materials on the building plans. | Building plans | Plan check prior to issuance of building permit | Review and approve building plans for inclusion of features | Planning | _____ | _____ |
| Air Quality | | | | | | |
| BECSP MM4.2-1 Project applicants shall require by contract specifications that all diesel-powered equipment used will be retrofitted with after-treatment products (e.g., engine catalysis). Contract specifications shall be included in project construction documents, which shall be reviewed by the City of Huntington Beach prior to issuance of a grading permit. | Contract language and notes on grading plans | Plan check prior to issuance of a grading permit | Review and approve grading plans for inclusion | Planning | _____ | _____ |
| BECSP MM4.2-2 Project applicants shall require by contract specifications that all heavy-duty diesel-powered equipment operating and refueling at the project site use low-NO _x diesel fuel to the extent that it is readily available and cost effective (up to 125 percent of the cost of California Air Resources Board diesel) in the South Coast Air Basin (this does not apply to diesel-powered trucks traveling to and from the project site). Contract specifications shall be included in project construction documents, which shall be reviewed by the City of Huntington Beach prior to issuance of a grading permit. | Contract language and notes on grading plans | Plan check prior to issuance of a grading permit | Review and approve grading plans for inclusion | Planning | _____ | _____ |
| BECSP MM4.2-3 Project applicants shall require by contract specifications that construction equipment engines be maintained in good condition and in proper tune per manufacturer's specification for the duration of project construction. Contract specifications shall be included in project construction documents, which shall be reviewed by the City of Huntington Beach prior to issuance of a grading permit. | Contract language and notes on grading plans | Plan check prior to issuance of a grading permit | Review and approve grading plans for inclusion | Planning | _____ | _____ |
| BECSP MM4.2-4 Project applicants shall require by contract specifications that construction operations rely on the electricity infrastructure surrounding the construction site rather than electrical generators powered by internal combustion engines. Contract specifications shall be included in project construction documents, which shall be reviewed by the City of Huntington Beach prior to issuance of a grading permit. | Contract language and notes on grading plans | Plan check prior to issuance of a grading permit | Review and approve grading plans for inclusion | Planning | _____ | _____ |

Table 1 (Continued)
Mitigation Monitoring and Reporting Program

| Mitigation Measure | Implementation Documentation | Timing | Monitoring Activity | Responsible Monitor | Compliance Verification Signature | Date |
|--|--|---|--|---------------------|-----------------------------------|------|
| <p>BECSP MM4.2-5 As required by South Coast Air Quality Management District Rule 403—Fugitive Dust, all construction activities that are capable of generating fugitive dust are required to implement dust control measures during each phase of project development to reduce the amount of particulate matter entrained in the ambient air. These measures include the following:</p> <ul style="list-style-type: none"> ■ Application of soil stabilizers to inactive construction areas ■ Quick replacement of ground cover in disturbed areas ■ Watering of exposed surfaces three times daily ■ Watering of all unpaved haul roads three times daily ■ Covering all stock piles with tarp ■ Reduction of vehicle speed on unpaved roads ■ Post signs on-site limiting traffic to 15 miles per hour or less ■ Sweep streets adjacent to the project site at the end of the day if visible soil material is carried over to adjacent roads ■ Cover or have water applied to the exposed surface of all trucks hauling dirt, sand, soil, or other loose materials prior to leaving the site to prevent dust from impacting the surrounding areas ■ Install wheel washers where vehicles enter and exit unpaved roads onto paved roads to wash off trucks and any equipment leaving the site each trip | <p>Contract language and notes on grading plans</p> | <p>Plan check prior to issuance of a grading permit</p> | <p>Review and approve grading plans for inclusion</p> | <p>Planning</p> | | |
| <p>BECSP MM4.2-6 Project applicants shall require by contract specifications that construction-related equipment, including heavy-duty equipment, motor vehicles, and portable equipment, shall be turned off when not in use for more than 30 minutes. Diesel-fueled commercial motor vehicles with gross vehicular weight ratings of greater than 10,000 pounds shall be turned off when not in use for more than 5 minutes. Contract specifications shall be included in the proposed project construction documents, which shall be approved by the City of Huntington Beach.</p> | <p>Contract language and notes on grading plans and construction plans</p> | <p>Plan check prior to issuance of a grading permit</p> | <p>Review and approve grading plans and building plans for inclusion</p> | <p>Planning</p> | | |
| <p>BECSP MM4.2-7 Project applicants shall require by contract specifications that construction parking be configured to minimize traffic interference during the construction period and, therefore, reduce idling of traffic. Contract specifications shall be included in the proposed project construction documents, which shall be approved by the City of Huntington Beach.</p> | <p>Contract language and notes on grading plans and construction plans</p> | <p>Plan check prior to issuance of a grading permit</p> | <p>Review and approve grading plans and building plans for inclusion</p> | <p>Planning</p> | | |

Table 1 (Continued)
Mitigation Monitoring and Reporting Program

| Mitigation Measure | Implementation Documentation | Timing | Monitoring Activity | Responsible Monitor | Compliance Verification Signature | Date |
|---|---|--|---|---------------------|-----------------------------------|------|
| <p>Beach.</p> <p>BECSP MM4.2-8 Project applicants shall require by contract specifications that temporary traffic controls are provided, such as a flag person, during all phases of construction to facilitate smooth traffic flow. Contract specifications shall be included in the proposed project construction documents, which shall be approved by the City of Huntington Beach.</p> | Contract language and notes on grading plans and construction plans | Plan check prior to issuance of a grading permit | Review and approve grading plans and building plans for inclusion | Planning | | |
| <p>BECSP MM4.2-9 Project applicants shall require by contract specifications that construction activities that affect traffic flow on the arterial system be scheduled to off-peak hours (10:00 A.M. to 4:00 P.M.). Contract specifications shall be included in the proposed project construction documents, which shall be approved by the City of Huntington Beach.</p> | Contract language and notes on grading plans and construction plans | Plan check prior to issuance of a grading permit | Review and approve grading plans and building plans for inclusion | Planning | | |
| <p>BECSP MM4.2-10 Project applicants shall require by contract specifications that dedicated on-site and off-site left-turn lanes on truck hauling routes be utilized for movement of construction trucks and equipment on site and off site to the extent feasible during construction activities. Contract specifications shall be included in the proposed project construction documents, which shall be approved by the City of Huntington Beach.</p> | Contract language and notes on grading plans and construction plans | Plan check prior to issuance of a grading permit | Review and approve grading plans and building plans for inclusion | Planning | | |
| <p>BECSP MM4.2-11 Upon issuance of building or grading permits, whichever is issued earlier, notification shall be mailed to owners and occupants of all developed land uses within 300 feet of a project site within the Specific Plan providing a schedule for major construction activities that will occur through the duration of the construction period. In addition, the notification will include the identification and contact number for a community liaison and designated construction manager that would be available on site to monitor construction activities. The construction manager shall be responsible for complying with all project requirements related to PM₁₀ generation. The construction manager will be located at the on-site construction office during construction hours for the duration of all construction activities. Contract information for the community liaison and construction manager will be located at the construction office, City Hall, the police department, and a sign on site.</p> | Mail to owners & occupants within 300 feet of project site a notice regarding major construction activities | Plan check prior to issuance of a grading or building permits, which occur earlier | Review and approve notice | Planning | | |
| <p>BECSP MM4.2-12 Project applicants shall require by contract specifications that the architectural coating (paint and primer) products</p> | Contract language and notes on | Plan check prior to | Review and approve building | Planning | | |

Table 1 (Continued)
Mitigation Monitoring and Reporting Program

| <i>Mitigation Measure</i> | <i>Implementation Documentation</i> | <i>Timing</i> | <i>Monitoring Activity</i> | <i>Responsible Monitor</i> | <i>Compliance Verification Signature</i> | <i>Date</i> |
|---|---|---|---|----------------------------|--|-------------|
| used would have a VOC rating of 125 grams per liter or less. Contract specifications shall be included in the proposed project construction documents, which shall be reviewed and approved by the City of Huntington Beach. | construction plans | issuance of a building permit | plans for inclusion | | | |
| BECSP MM4.2-13 Project applicants shall require by contract specifications that materials that do not require painting be used during construction to the extent feasible. Contract specifications shall be included in the proposed project construction documents, which shall be reviewed and approved by the City of Huntington Beach. | Contract language and notes on grading plans and construction plans | Plan check prior to issuance of a grading permit | Review and approve grading plans and construction plans for inclusion | Planning | | |
| BECSP MM4.2-14 Project applicants shall require by contract specifications that pre-painted construction materials be used to the extent feasible. Contract specifications shall be included in the proposed project construction documents, which shall be reviewed and approved by the City of Huntington Beach. | Contract language and notes on grading plans and construction plans | Plan check prior to issuance of a grading permit | Review and approve grading plans for inclusion | Planning | | |
| Global Climate Change | | | | | | |
| BECSP MM4.15-1 The City shall require by contract specifications that all diesel-powered equipment used would be retrofitted with after-treatment products (e.g., engine catalysts and other technologies available at the time construction commences) to the extent that they are readily available and cost effective when construction activities commence. Contract specifications shall be included in the proposed project construction documents, which shall be approved by the City of Huntington Beach. | Contract language and notes on grading plans and construction plans | Plan check prior to issuance of a grading permit | Review and approve grading plans for inclusion | Planning | | |
| BECSP MM4.15-2 The City shall require by contract specifications that alternative fuel construction equipment (i.e., compressed natural gas, liquid petroleum gas, and unleaded gasoline) would be utilized to the extent feasible at the time construction activities commence. Contract specifications shall be included in the proposed project construction documents, which shall be approved by the City of Huntington Beach. | Contract language and notes on grading plans and construction plans | Plan check prior to issuance of a grading permit | Review and approve grading plans for inclusion | Planning | | |
| BECSP MM4.15-3 The City shall require by contract specifications that developers within the project site use locally available building materials, to the extent feasible, such as concrete, stucco, and interior finishes, for construction of the project and associated infrastructure. | Contract language and notes on construction plans | Plan check prior to issuance of a building permit | Review and approve building plans for inclusion | Planning | | |

Table 1 (Continued)
Mitigation Monitoring and Reporting Program

| <i>Mitigation Measure</i> | <i>Implementation Documentation</i> | <i>Timing</i> | <i>Monitoring Activity</i> | <i>Responsible Monitor</i> | <i>Compliance Verification Signature</i> | <i>Date</i> |
|--|--|---|--|----------------------------|--|--------------|
| <p>BECSP MM4.15-4 The City shall require developers within the project site to establish a construction management plan with Rainbow Disposal to divert a target of 50 percent of construction, demolition, and site clearing waste.</p> | <p>Contract language and notes on grading plans and construction plans</p> | <p>Plan check prior to issuance of a grading or building permit, which occurs earlier</p> | <p>Review and approve grading plans and building plans for inclusion</p> | <p>Planning</p> | <p>_____</p> | <p>_____</p> |
| <p>BECSP MM4.15-5 The City shall require by contract specifications that construction equipment engines will be maintained in good condition and in proper tune per manufacturer's specification for the duration of construction. Contract specifications shall be included in the proposed project construction documents, which shall be approved by the City of Huntington Beach.</p> | <p>Contract language and notes on grading plans and construction plans</p> | <p>Plan check prior to issuance of a grading permit</p> | <p>Review and approve grading plans and building plans for inclusion</p> | <p>Planning</p> | <p>_____</p> | <p>_____</p> |
| <p>BECSP MM4.15-6 The City shall require by contract specifications that construction-related equipment, including heavy-duty equipment, motor vehicles, and portable equipment, shall be turned off when not in use for more than five minutes. Diesel-fueled commercial motor vehicles with gross vehicular weight ratings of greater than 10,000 pounds shall be turned off when not in use for more than five minutes. Contract specifications shall be included in the proposed project construction documents, which shall be approved by the City of Huntington Beach.</p> | <p>Contract language and notes on grading plans and construction plans</p> | <p>Plan check prior to issuance of a grading permit</p> | <p>Review and approve grading plans and building plans for inclusion</p> | <p>Planning</p> | <p>_____</p> | <p>_____</p> |
| <p>BECSP MM4.15-7 The City shall require that any new development within the Specific Plan area provide signs within loading dock areas clearly visible to truck drivers. These signs shall state that trucks cannot idle in excess of five minutes per trip.</p> | <p>Contract language and notes on construction plans</p> | <p>Plan check prior to issuance of a building permit</p> | <p>Review and approve building plans for inclusion</p> | <p>Planning</p> | <p>_____</p> | <p>_____</p> |
| <p>BECSP MM4.15-8 The City shall require by contract specifications that electrical outlets are included in the building design of future loading docks to allow use by refrigerated delivery trucks. Future project-specific Applicants shall require that all delivery trucks do not idle for more than five minutes. If loading and/or unloading of perishable goods would occur for more than five minutes, and continual refrigeration is required, all refrigerated delivery trucks shall use the electrical outlets to continue</p> | <p>Contract language and notes on construction plans</p> | <p>Plan check prior to issuance of a building permit</p> | <p>Review and approve building plans for inclusion</p> | <p>Planning</p> | <p>_____</p> | <p>_____</p> |

Table 1 (Continued)
Mitigation Monitoring and Reporting Program

| Mitigation Measure | Implementation Documentation | Timing | Monitoring Activity | Responsible Monitor | Compliance Verification Signature | Date |
|--|--|---|---|---------------------|-----------------------------------|------|
| <p>powering the truck refrigeration units when the delivery truck engine is turned off.</p> <p>BECSP MM4.15-9 The City shall require that any new development within the project site provide a bulletin board or kiosk in the lobby of each proposed structure that identifies the locations and schedules of nearby transit opportunities.</p> | <p>Contract language and notes on construction plans</p> | <p>Plan check prior to issuance of certificate of occupancy</p> | <p>Review and approve building plans for inclusion</p> | <p>Planning</p> | | |
| <p>Hydrology and Water Quality</p> | | | | | | |
| <p>BECSP MM4.7-1 City of Huntington Beach shall require Applicants for new development and significant redevelopment projects within the Specific Plan area to prepare a project Water Quality Management Plan (WQMP) in accordance with the DAMP requirements and measures described below and with all current adopted permits. The WQMP shall be prepared by a Licensed Civil Engineer and submitted for review and acceptance prior to issuance of a Precise Grading or Building permit.</p> <p>BMPs in the WQMP shall be designed in accordance with the Municipal NPDES Permit, Model WQMP, DAMP, and City of Huntington Beach LIP. As noted in the Specific Plan, all development projects shall include site design and source control BMPs in the project WQMP. Additionally, new development or significant redevelopment projects and priority projects shall include LID principles to reduce runoff to a level consistent with the maximum extent practicable and treatment control BMPs in the WQMP.</p> <p>If permanent dewatering is required and allowed by the City, OCWD, and other regulatory agencies, the Applicant shall include a description of the dewatering technique, discharge location, discharge quantities, chemical characteristics of discharged water, operations and maintenance plan, and WDID number for proof of coverage under the De Minimus Threat General Permit or copy of the individual WDR in the WQMP. Additionally, the WQMP shall incorporate any additional BMPs as required by the City Public Works Department.</p> <p>The WQMP shall include the following additional requirements: Project and Site Characterization Requirements</p> <ul style="list-style-type: none"> Entitlement Application numbers and site address shall be included on the title sheet of the WQMP | <p>Water Quality Management Plan</p> | <p>Prior to receiving a precise grading permit</p> | <p>Review and approve Water Quality Management Plan</p> | <p>Public Works</p> | | |

Table 1 (Continued)

Mitigation Monitoring and Reporting Program

| Mitigation Measure | Implementation Documentation | Timing | Monitoring Activity | Responsible Monitor | Compliance Verification Signature | Date |
|---|------------------------------|--------|---------------------|---------------------|-----------------------------------|------|
| <ul style="list-style-type: none"> ■ In the project description section, explain whether proposed use includes onsite food preparation, eating areas (if not please state), outdoor activities to be expected, vehicle maintenance, service, washing cleaning (if prohibited onsite, please state) ■ All potential pollutants of concern for the proposed project land use type as per Table 7.11-1 of the Orange County Model Water Quality Management Plan shall be identified ■ A narrative describing how all potential pollutants of concern will be addressed through the implementation of BMPs and describing how site design BMP concepts will be considered and incorporated into the project design shall be included ■ Existing soil types and estimated percentages of perviousness for existing and proposed conditions shall be identified ■ In Section I of the WQMP, state verbatim the Development Requirements from the Planning Department's letter to the Applicant ■ A site plan showing the location of the selected treatment control BMPs and drainage areas shall be included in the WQMP ■ A Geotechnical Report shall be submitted to address site conditions for determination of infiltration limitations and other pertinent characteristics. | | | | | | |
| <p><u>Project-Based Treatment Control BMPs</u></p> <ul style="list-style-type: none"> ■ Infiltration-type BMPs shall not be used unless the Geotechnical Report states otherwise. Depth to seasonal high groundwater is determined to provide at least a 10-foot clearance between the bottom of the BMP and top of the water table. It is expected that infiltration BMPs may be feasible between Holland Drive and Utica Drive, however, a Geotechnical Investigation must be conducted to ensure sufficient properties ■ Wet swales and grassed channels shall not be used because of the slow infiltration rates of project site soils, the potentially shallow depth to groundwater, and water conservation needs ■ If proprietary Structural Treatment Control devices are used, they shall be sited and designed in compliance with the manufacturers design criteria ■ Surface exposed treatment control BMPs shall be selected such that | | | | | | |

Table 1 (Continued)

Mitigation Monitoring and Reporting Program

| Mitigation Measure | Implementation Documentation | Timing | Monitoring Activity | Responsible Monitor | Compliance Verification Signature | Date |
|--|------------------------------|--------|---------------------|---------------------|-----------------------------------|------|
| <p>standing water drains or evaporates within 24 hours or as required by the County's vector control</p> <ul style="list-style-type: none"> ■ Excess stormwater runoff shall bypass the treatment control BMPs unless they are designed to handle the flow rate or volume from a 100-year storm event without reducing effectiveness. Effectiveness of any treatment control BMP for removing the pollutants of concern shall be documented via analytical models or existing studies on effectiveness. ■ The project WQMP shall incorporate water efficient landscaping using drought tolerant, native plants in accordance with Landscape and Irrigation Plans as set forth by the Association (see below) ■ Pet waste stations (stations that provide waste pick-up bags and a convenient disposal container protected from precipitation) shall be provided and maintained ■ Building materials shall minimize exposure of bare metals to stormwater. Copper or Zinc roofing materials, including downspouts, shall be prohibited. Bare metal surfaces shall be painted with non-lead-containing paint <p>The following BMPs shall not be used because they have not been shown to be effective in many situations. Therefore, unless sufficient objective studies and review are available and supplied with the WQMP to correctly size devices and to document expected pollutant removal rates the WQMP shall not include:</p> <ul style="list-style-type: none"> ■ Hydrodynamic separator type devices as a BMP for removing any pollutant except trash and gross particulates ■ Oil and Grit separators ■ Any Applicant proposing development in the Specific Plan Area is encouraged to consider the following BMPs: ■ Sand filters or other filters (including media filters) for rooftop runoff ■ Dry swales. A dry swale treatment system could be used if sufficient area, slope gradient, and length of swale could be incorporated into the project design. Dry swales could remove substantial amounts of nutrients, suspended solids, metals, and petroleum hydrocarbons ■ Other proprietary treatment devices (if supporting documentation is provided) | | | | | | |

Table 1 (Continued)
Mitigation Monitoring and Reporting Program

| Mitigation Measure | Implementation Documentation | Timing | Monitoring Activity | Responsible Monitor | Compliance Verification Signature | Date |
|--|------------------------------|--------|---------------------|---------------------|-----------------------------------|------|
| <p><u>Non-Structural BMPs</u></p> <p>The WQMP shall include the following operations and maintenance BMPs under the management of a Homeowners/Business Association (Association), where applicable. The Association shall fund and implement an operational and maintenance program that includes the following:</p> <ul style="list-style-type: none"> ■ The Association shall dictate minimum landscape maintenance standards and tree trimming requirements for the total project site. Landscape maintenance shall be performed by a qualified landscape maintenance company or individual in accordance with a Chemical Management Plan detailing chemical application methods, chemical handling procedures, and worker training. Pesticide application shall be performed by a certified applicator. No chemicals shall be stored on-site unless in a covered and contained area and in accordance with an approved Materials Management Plan. Application rates shall not exceed labeled rates for pesticides, and shall not exceed soil test rates for nutrients. Slow release fertilizers shall be used to prevent excessive nutrients in stormwater or irrigation runoff. ■ The Association shall have the power and duty to establish, oversee, guide, and require proper maintenance and tree trimming procedures per the ANSI A-300 Standards as established by the International Society of Arborist. The Association shall require that all trees be trimmed by or under the direct observation/direction of a licensed/certified Arborist for the entire area. The Association shall establish minimum standards for maintenance for the total community, and establish enforcement thereof for the total community. The Association shall rectify problems arising from incorrect tree trimming, chemical applications, and other maintenance within the total community. ■ Landscape irrigation shall be performed in accordance with an Irrigation Management Plan to minimize excess irrigation contributing to dry- and wet-weather runoff. Automated sprinklers shall be used and be inspected at least quarterly and adjusted yearly to minimize potential excess irrigation flows. Landscape irrigation maintenance shall be performed in accordance with the approved irrigation plans, the City Water Ordinance and per the City Arboricultural and Landscape Standards and Specifications. | | | | | | |

ATTACHMENT NO. 8.10

Table 1 (Continued)
Mitigation Monitoring and Reporting Program

| Mitigation Measure | Implementation Documentation | Timing | Monitoring Activity | Responsible Monitor | Compliance Verification Signature | Date |
|--|------------------------------|--------|---------------------|---------------------|-----------------------------------|------|
| <ul style="list-style-type: none"> ■ Proprietary stormwater treatment systems maintenance shall be in accordance with the manufacturer's recommendations. If a nonproprietary treatment system is used, maintenance shall be in accordance with standard practices as identified in the current CASQA (2003) handbooks, operations and maintenance procedures outlined in the approved WQMP, City BMP guidelines, or other City-accepted guidance. ■ Signage, enforcement of pet waste controls, and public education would improve use and compliance, and therefore, effectiveness of the program, and reduce the potential for hazardous materials and other pollution in stormwater runoff. The Association shall prepare and install appropriate signage, disseminate information to residents and retail businesses, and include pet waste controls (e.g., requirements for pet waste cleanup, pet activity area restrictions, pet waste disposal restrictions) in the Association agreement/Conditions, Covenants, and Restrictions. ■ Street sweeping shall be performed at an adequate frequency to prevent build up of pollutants (see http://www.fhwa.dot.gov/environment/ultraurb/ for street sweeping effectiveness). ■ The Association shall develop a maintenance plan for BMPs and facilities identifying responsible parties and maintenance schedules and appropriate BMPs to minimize discharges of contaminants to storm drain systems during maintenance operations. ■ Reporting requirements: the Association shall prepare an annual report and submit the annual report to the City of Huntington Beach documenting the BMPs operations and maintenance conducted that year. The annual report shall also address the potential system deficiencies and corrective actions taken or planned. <p><u>Site Design BMPs</u> Any Applicant proposing development in the Specific Plan Area is required to incorporate LID principles as defined in the Municipal NPDES Permit and is encouraged to consider the following BMPs, if allowed in accordance with the Geotechnical Report and limitations on infiltration BMPs:</p> | | | | | | |

Table 1 (Continued)
Mitigation Monitoring and Reporting Program

| Mitigation Measure | Implementation Documentation | Timing | Monitoring Activity | Responsible Monitor | Compliance Verification Signature | Date |
|--|------------------------------|---|--|---------------------|-----------------------------------|------|
| <ul style="list-style-type: none"> ■ Use of porous concrete or asphalt (if acceptable to the Geotechnical Engineer and where infiltration will not adversely affect groundwater) or other pervious pavement for driveways, paths, sidewalks, and courtyards/open space areas, to the maximum extent practicable, would reduce pollutants in stormwater runoff as well as provide some detention within the material void¹ space. If porous paver blocks are used, they shall be adequately maintained to provide continued porosity (effectiveness) ■ Incorporation of rain gardens or cisterns to reuse runoff for landscape irrigation ■ Green roofs to reduce runoff and treat roof pollutants <p>Site design and landscape planning to group water use requirements for efficient irrigation</p> | | | | | | |
| <p>BECSP MM4.7-2 The City of Huntington Beach shall require that any Applicant prepare a Groundwater Hydrology Study to determine the lateral transmissivity of area soils and a safe pumping yield such that dewatering activities do not interfere with nearby water supplies. The Groundwater Hydrology Study shall make recommendations on whether permanent groundwater dewatering is feasible within the constraints of a safe pumping level. The Applicant's engineer of record shall incorporate the Hydrology Study designs and recommendations into project plans. If safe groundwater dewatering is determined to not be feasible, permanent groundwater dewatering shall not be implemented. The City Director of Public Works, OCWD, and other regulatory agencies shall approve or disapprove any permanent groundwater dewatering based on the Groundwater Hydrology Study and qualified Engineers' recommendations.</p> | Groundwater Hydrology Study | Prior to issuance of a precise grading permit | Review and approve Groundwater Hydrology Study, if necessary | Public Works | | |

¹ Void space is the empty space between individual particles.

Table 1 (Continued)
Mitigation Monitoring and Reporting Program

| Mitigation Measure | Implementation Documentation | Timing | Monitoring Activity | Responsible Monitor | Compliance Verification Signature | Date |
|--|---|--|--|---|-----------------------------------|------|
| <p>BECSP MMA.7-3 The City of Huntington Beach shall require that the Applicant's Licensed Civil Engineer for each site-specific development prepare a Hydrology and Hydraulic Study to identify the effects of potential stormwater runoff from the specific development on the existing storm drain flows for the 10-, 25-, and 100-year design storm events. The Hydrology and Hydraulic Study shall identify existing runoff and proposed runoff, in addition to existing storm drain system capacity at the development site discharge location to the nearest down-gradient main junction. The Applicant shall design site drainage and document that the proposed development would not increase peak storm event flows over existing conditions for the design storm events. The final site plan shall not exceed an impervious fraction of 0.9, unless sufficient retention is incorporated into the site design to accommodate excess runoff. The Hydrology and Hydraulic Study shall also incorporate all current adopted Municipal NPDES Permit requirements for stormwater flow calculations and retention/detention features in effect at the time of review.</p> | <p>Hydrology and Hydraulics Study</p> <p>Precise final grading and street improvement plans and studies</p> | <p>Prior to issuance of a precise grading permit</p> <p>Following grading, excavation, and installation of utilities</p> | <p>Review and approve Hydrology and Hydraulics Study</p> <p>Inspect improvements; verify implementation per approved plans</p> | <p>Public Works</p> <p>Public Works</p> | | |
| <p>BECSP MMA.7-4 The City of Huntington Beach shall require that adequate capacity in the storm drain system is demonstrated from the specific development site discharge location to the nearest main channel to accommodate discharges from the specific development. If capacity is demonstrated as adequate, no upgrades will be required. If capacity is not adequate, the City of Huntington Beach shall identify corrective action(s) required by the specific development Applicant to ensure adequate capacity. Corrective action could include, but is not limited to:</p> <ul style="list-style-type: none"> ■ Construction of new storm drains, as identified in the MPD or based on the Hydrology and Hydraulic Study, if the Hydrology and Hydraulic Study identifies greater impacts than the MPD ■ Improvement of existing storm drains, as identified in the MPD or based on the Hydrology and Hydraulic Study, if the Hydrology and Hydraulic Study identifies greater impacts than the MPD ■ In-lieu fees to implement system-wide storm drain infrastructure improvements ■ Other mechanisms as determined by the City Department of Public Works. ■ For nonresidential areas, if redevelopment would result in an | <p>Hydrology and Hydraulics Study</p> | <p>Prior to issuance of a precise grading permit</p> | <p>Review and approve Hydrology and Hydraulics Study</p> | <p>Public Works</p> | | |

ATTACHMENT NO. 8.13

Table 1 (Continued)
Mitigation Monitoring and Reporting Program

| Mitigation Measure | Implementation Documentation | Timing | Monitoring Activity | Responsible Monitor | Compliance Verification Signature | Date |
|---|--|---|--|---------------------------------|-----------------------------------|------|
| <p>impervious fraction of less than 0.9 and does not increase the directly connected impervious area compared to existing conditions, runoff is expected to remain the same or less than as assessed in the MPD and only MPD improvements would be required.</p> <p>Because some storm drain system constraints may be located far downgradient from the actual development site, several properties may serve to contribute to system capacity constraints. Therefore, the City Department of Public Works shall assess each site development and system characteristics to identify the best method for achieving adequate capacity in the storm drain system. Drainage assessment fees/districts to improve/implement storm drains at downstream locations or where contributing areas are large are enforced through <i>Municipal Code</i> (Section 14.20).</p> <p>The City Department of Public Works shall review the Hydrology and Hydraulic Study and determine required corrective action(s) or if a waiver of corrective action is applicable. The site-specific development Applicant shall incorporate required corrective actions into their project design and/or plan. Prior to receiving a Certificate of Occupancy or final inspection, the City Department of Public Works shall ensure that required corrective action has been implemented.</p> | | | | | | |
| <p>Noise</p> <p>BECS M4.9-1 Project applicants shall require by contract specifications that the following construction best management practices (BMPs) be implemented by contractors to reduce construction noise levels:</p> <ul style="list-style-type: none"> ■ Two weeks prior to the commencement of construction, notification must be provided to surrounding land uses within 300 feet of a project site disclosing the construction schedule, including the various types of activities that would be occurring throughout the duration of the construction period ■ Ensure that construction equipment is properly muffled according to industry standards and be in good working condition ■ Place noise-generating construction equipment and locate construction staging areas away from sensitive uses, where feasible ■ Schedule high noise-producing activities between the hours of 8:00 A.M. and 5:00 P.M. to minimize disruption on sensitive uses, | <p>Contract language and notes on grading and building plans</p> | <p>Plan check prior to issuance of a grading permit</p> | <p>Review and approve grading plans and building plans for inclusion</p> <p>Periodic field check</p> | <p>Planning</p> <p>Planning</p> | | |

ATTACHMENT NO. 8.14

Table 1 (Continued)

Mitigation Monitoring and Reporting Program

| Mitigation Measure | Implementation Documentation | Timing | Monitoring Activity | Responsible Monitor | Compliance Verification Signature | Date |
|---|--|--|--|---------------------|-----------------------------------|------|
| <p>Monday through Saturday. Schedule pile-driving activities between the hours of 8:00 A.M. and 4:00 P.M. on Mondays through Fridays only.</p> <ul style="list-style-type: none"> ■ Implement noise attenuation measures, which may include, but are not limited to, temporary noise barriers or noise blankets around stationary construction noise sources ■ Use electric air compressors and similar power tools rather than diesel equipment, where feasible ■ Construction-related equipment, including heavy-duty equipment, motor vehicles, and portable equipment, shall be turned off when not in use for more than 10 minutes ■ Construction hours, allowable workdays, and the phone number of the job superintendent shall be clearly posted at all construction entrances to allow for surrounding owners and residents to contact the job superintendent. If the City or the job superintendent receives a complaint, the superintendent shall investigate, take appropriate corrective action, and report the action taken to the reporting party. <p>Contract specifications shall be included in the proposed project construction documents, which shall be reviewed by the City prior to issuance of a grading permit.</p> | | | | | | |
| <p>BECSP MIM4.9-2 Project applicants shall require by contract specifications that construction staging areas along with the operation of earthmoving equipment within the project area would be located as far away from vibration and noise sensitive sites as possible. Contract specifications shall be included in the proposed project construction documents, which shall be reviewed by the City prior to issuance of a grading permit.</p> | <p>Contract language and notes on grading plans and building plans</p> | <p>Prior to issuance of a grading permit</p> | <p>Review and approve grading plans and building plans for inclusion</p> | <p>Planning</p> | | |
| <p>BECSP MIM4.9-3 Project applicants shall require by contract specifications that heavily loaded trucks used during construction would be routed away from residential streets. Contract specifications shall be included in the proposed project construction documents, which shall be reviewed by the City prior to issuance of a grading permit.</p> | <p>Contract language and notes on grading plans and building plans</p> | <p>Prior to issuance of a grading permit</p> | <p>Review and approve grading plans and building plans for inclusion</p> | <p>Planning</p> | | |

Table 1 (Continued)

Mitigation Monitoring and Reporting Program

| Mitigation Measure | Implementation Documentation | Timing | Monitoring Activity | Responsible Monitor | Compliance Verification Signature | Date |
|---|--|---|----------------------|---------------------|-----------------------------------|------|
| MM 4.F-1 Generators shall be equipped with a radiator silencer to minimize noise. | Written proof from manufacturer and/or company providing equipment of installation of silencer | Prior to each special event | Review documentation | Planning | | |
| Traffic/Transportation | | | | | | |
| BECSP MM4.13-1 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the addition of a separate westbound right turn lane to the intersection of Beach Boulevard at Warner Avenue. Implementation of this improvement would require Caltrans approval. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | | |
| BECSP MM4.13-2 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the addition of dual northbound and southbound left turn lanes to the intersection of Beach Boulevard at Garfield Avenue. Implementation of this improvement would require Caltrans approval. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | | |
| BECSP MM4.13-3 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the addition of a fourth northbound through lane to the intersection of Brookhurst Street at Adams Avenue. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | | |
| BECSP MM4.13-4 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the addition of a separate northbound right turn lane to the intersection of Brookhurst Street at Adams Avenue. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | | |
| BECSP MM4.13-5 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the addition of a fourth southbound through lane to the intersection of Brookhurst Street at Adams Avenue. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | | |
| BECSP MM4.13-6 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the addition of a fourth eastbound through lane to the intersection of Brookhurst Street at Adams Avenue. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | | |

Table 1 (Continued)

Mitigation Monitoring and Reporting Program

| Mitigation Measure | Implementation Documentation | Timing | Monitoring Activity | Responsible Monitor | Compliance Verification Signature | Date |
|---|------------------------------|---|---------------------|---------------------|-----------------------------------|------|
| BECSP MM4.13-7 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the addition of a fourth westbound through lane to the intersection of Brookhurst Street at Adams Avenue. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | | |
| BECSP MM4.13-8 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution to allow a right turn overlap for a westbound right turn at the intersection of Brookhurst Street at Adams Avenue. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | | |
| BECSP MM4.13-9 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution to allow a right turn overlap for a northbound right turn at the intersection of Brookhurst Street at Adams Avenue. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | | |
| BECSP MM4.13-10 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the addition of a fourth northbound through lane to the intersection of Beach Boulevard at Edinger Avenue. Implementation of this improvement would require Caltrans approval. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | | |
| BECSP MM4.13-11 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the addition of a third westbound through lane to the intersection of Beach Boulevard at Edinger Avenue. Implementation of this improvement would require Caltrans approval. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | | |
| BECSP MM4.13-12 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the addition of a separate southbound right turn lane to the intersection of Beach Boulevard at Bolsa Avenue. Implementation of this improvement would require Caltrans and City of Westminster approvals. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | | |
| BECSP MM4.13-13 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the addition of a second westbound left turn lane to the intersection of Beach Boulevard at Talbert Avenue. Implementation of this improvement would require Caltrans approval. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | | |

Table 1 (Continued)
Mitigation Monitoring and Reporting Program

| <i>Mitigation Measure</i> | <i>Implementation Documentation</i> | <i>Timing</i> | <i>Monitoring Activity</i> | <i>Responsible Monitor</i> | <i>Compliance Verification Signature</i> | <i>Date</i> |
|--|-------------------------------------|---|----------------------------|----------------------------|--|-------------|
| BECSP MM4.13-14 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the addition of a de facto westbound right turn lane to the intersection of Beach Boulevard at Talbert Avenue. Implementation of this improvement would require Caltrans approval. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | _____ | _____ |
| BECSP MM4.13-15 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the conversion of a separate westbound right turn lane to a de facto right turn lane at the intersection of Newland Street at Warner Avenue. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | _____ | _____ |
| BECSP MM4.13-16 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the addition of a third westbound through lane to the intersection of Newland Street at Warner Avenue. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | _____ | _____ |
| BECSP MM4.13-17 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the addition of a separate southbound right turn lane to the intersection of Beach Boulevard at McFadden Avenue. Implementation of this improvement would require Caltrans and City of Westminster approvals. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | _____ | _____ |
| BECSP MM4.13-18 For future projects that occur within the Specific Plan area, the project applicant(s) shall make a fair share contribution for the addition of a separate northbound right turn lane to the intersection of Beach Boulevard at McFadden Avenue. Implementation of this improvement would require Caltrans and City of Westminster approvals. | Proof of fair share payment | Prior to issuance of certificate of occupancy | Confirm payment | Public Works | _____ | _____ |

Table 1 (Continued)
Mitigation Monitoring and Reporting Program

| Mitigation Measure | Implementation Documentation | Timing | Monitoring Activity | Responsible Monitor | Compliance Verification Signature | Date |
|--|--------------------------------|---|---|---------------------|-----------------------------------|--------------|
| <p>MM 4.G-1 Prior to special events, the skate park operator, in consultation with the City of Huntington Beach and the adjacent property owners, shall implement measures to the extent feasible to minimize the potential for off-site parking impacts to neighboring uses. This shall include obtaining all necessary permissions or permits as needed. These measures can include, but are not limited to, the following:</p> <ul style="list-style-type: none"> ■ Provide access management for the staging area, including personnel to guide/direct visitors to appropriate parking areas; ■ Provide management techniques for use of the overflow parking, including the use of valet parking in a portion of the remote lot (Huntington Beach Sports Complex); ■ Provide permit parking and supplemental patrol for residential neighborhoods adjacent to the project site at no cost to residents; and ■ Provide signage to direct visitors to the remote lot and discourage visitors from parking in adjacent residential neighborhoods and the Bella Terra commercial area; ■ Post "No Event Parking" signs or similar at entrances to adjacent lots and provide attendants or monitoring as requested or needed; and ■ Meet with adjacent property owners as needed to determine mitigation measure effectiveness and amend programs to address unforeseen impacts, as feasible. | <p>Parking Management Plan</p> | <p>Prior to the first special event</p> | <p>Review and approve Parking Management Plan</p> | <p>Public Works</p> | <p>_____</p> | <p>_____</p> |